

# Village of Hortonville Comprehensive Outdoor Recreation Plan

2015 - 2019



**VILLAGE OF HORTONVILLE**

**5-Year**

**COMPREHENSIVE OPEN SPACE AND OUTDOOR  
RECREATION PLAN**

**2015-2019**

**June 18, 2015**

Prepared by the

Hortonville Outdoor Recreation & Leisure Advisory Committee,

Hortonville Public Works Department,  
Carl McCrary, Director of Public Works

and the

East Central Wisconsin Regional Planning Commission  
Trish Nau, Principal Recreation Planner

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## **MISSION**

*The Village of Hortonville is committed to investing in a safe and welcoming community dedicated to promoting a sense of hometown identity and pride, while creating opportunities for civic and neighborhood involvement.*

**Hortonville Outdoor Recreation & Leisure Advisory Committee** is composed of approximately 12 members, made up of Village Board Members, Youth Sports, Public Works Staff, and Citizens who were interested in serving.

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## ABSTRACT

**TITLE:** Village of Hortonville  
Comprehensive Outdoor Recreation Plan 2015-2019

**CONTACT:** Trish Nau, ECWRPC Principal Planner

**AUTHORS:** Village of Hortonville Outdoor Recreation and Leisure  
Advisory Committee (ORLAC)  
Carl McCrary, Village Public Works Director  
Trish Nau, ECWRPC Principal Planner

**SUBJECT:** Village of Hortonville Comprehensive Outdoor  
Recreation Plan, (CORP) 5-Year update

**DATE:** Adopted by the Outdoor Recreation Leisure Advisory  
Committee and Village Board on June 18, 2015

**PLANNING AGENCY:** East Central Wisconsin Regional Planning Commission

**SOURCE OF COPIES:** East Central Wisconsin Regional Planning Commission  
400 Ahnaip Street, Suite 100  
Menasha, WI 54952  
(920) 751-4770  
[www.ecwrpc.org](http://www.ecwrpc.org)

The 2015-2019 version of Hortonville's Comprehensive Outdoor Recreation Plan seeks to help preserve the Village's natural environment and at the same time allow residents and visitors alike to play, learn, enjoy and live in harmony with it. In addition to setting forth new recommendations based on present needs, the adopted plan will enable the Village to once again compete for matching funds available through the Department of Natural Resources' Stewardship Program if available. Funding components of this program, targets monies for parkland acquisition and development projects as well as for numerous other projects and activities that preserve, protect and enhance important land and water-based natural assets. Collectively, other grants are available through the WDNR Stewardship Program and have effectively doubled the local funding commitment for acquisition and development of parkland and other public recreational facilities.

Village of Hortonville  
531 N. Nash Street  
Hortonville, WI 54944  
(920) 779-6011  
Web: <http://www.hortonvillewi.org/>





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## **CHAPTER 1**

### **INTRODUCTION**

## CHAPTER 1: INTRODUCTION

### THE VILLAGE OF HORTONVILLE

The Village of Hortonville is located in southwest Outagamie County at the intersection of STH 15 and CTH M, 10 miles northwest of the City of Appleton (Figure 1-1). It is bordered by the Town of Greenville to the southeast, Town of Ellington to the northeast, Town of Dale to the south and Town of Horton to the north and west. Approximately 2,711<sup>1</sup> people live in the Village that encompasses about 2,213 acres<sup>2</sup>. Between 2000 (2,357) and 2010 (2,711), the Village experienced modest population growth with its population growing by 15 percent during this time period. At the same time, the Village's land area increased by approximately 22.6 percent due to numerous annexations<sup>3</sup>.

**Figure 1-1: Location Map**



On August 7, 2013, the Village was struck by a devastating tornado. While miraculously no one was killed or injured, the tornado left a trail of destruction that will impact the Village for many years to come. Many old trees were uprooted and homes, businesses and public infrastructure were damaged.

In 2018, construction will start on the Wisconsin State Highway 15 Bypass around Hortonville. As part of the project, roundabouts will be constructed east and west of the Village, Main Street will be reconstructed and a jurisdictional transfer of old STH 15 (Main Street) through Hortonville from the State of Wisconsin to Outagamie County will occur. The transfer of traffic to the

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<sup>1</sup> U.S. Census, 2010.

<sup>2</sup> East Central Wisconsin Regional Planning Commission, Land Use Inventory, 2013.

<sup>3</sup> U.S. Census, 408 acres annexed between 2001 and 2005.



bypass and the reconstruction of Main Street will provide an opportunity for the Village to incorporate design elements that advances the Village's vision.

Rebuilding following the tornado and the reconstruction and jurisdictional transfer of Main Street will occur during the life of the comprehensive plan. The recommendations identified in this plan should be used by local officials to guide decisions in the coming years.

## HISTORY OF RECREATION PLANNING IN HORTONVILLE

Recreation has always played a major role in the lives of Hortonville residents. The community's natural amenities provide a wide range of recreational opportunities for resident and visitor alike.

Hortonville has a long history of planning for the recreational needs of its citizens. Working closely with the Village Board, efforts have focused on providing facilities and programs to meet changing recreational needs. This type of comprehensive recreational planning is an important tool in guiding the growth and development of Hortonville's park system. Recognizing the value of continuing this planning process, the Village, with assistance from ECWRPC, last updated its plan in 1983 and 1995. This process should be completed every 5 years to stay current with grant requirements.

This plan draws upon the goals, objectives, and recommendations from the Comprehensive Plan tempered with new considerations for just parks and recreation facilities which have evolved. During its preparation, many existing needs were reaffirmed, new needs identified, and an action program designed to fulfill these needs. The completed plan will serve to guide the development of Hortonville's park system for the next five years and beyond.

### Village of Hortonville Mission and Vision Statements

**Mission Statement:** *The Village of Hortonville is committed to investing in a safe and welcoming community dedicated to promoting a sense of hometown identity and pride, while creating opportunities for civic and neighborhood involvement.*

**Government Mission Statement:** *In partnership with the community, the Village of Hortonville is committed to providing high quality services and improving quality of life through thoughtful planning, fiscal responsibility, and accessibility, responsive, and proactive leadership.*

**Vision Statement:** *The Village of Hortonville is a vibrant, safe community offering a meaningful quality of life through:*

- *Visually attractive and inviting atmosphere,*
- *Events that celebrate community,*
- *Quality municipal services and facilities,*
- *Thriving business environment, and*
- *Diverse recreational opportunities.*

## PLAN PURPOSE AND PROCESS

The purpose of this plan is to develop an updated five-year and long-range action program that can continue to guide the growth of Hortonville's park system as the community expands and needs change. As a prelude for deriving the action program, the plan inventories existing parks and facilities, determines deficiencies and surpluses within the system, and identifies future recreational needs. These serve as the cornerstone for formulating general and site-specific recommendations, which provide the basis for all projects included in the action program. As mentioned earlier, a second purpose of the plan is to satisfy the mandates of available funding programs. Among these mandates are requirements that all funded projects must be clearly identified and justified in current plans to qualify. Once approved, the Village will once again be eligible for state-administered funding programs and cost sharing during the interval of 2015-2019.

The success of any type of local planning effort is often dependent on the involvement, support, and active participation of community residents in the plan's preparation. To ensure that a diverse spectrum of citizen viewpoints and concerns contributed to the plan's development, a broad base of citizen support and input was solicited from individuals representing a variety of interests through an online survey. Among the groups contacted were Senior Citizens, Native Americans, youth, and physically handicapped. Persons representing the business community and educational systems were also involved in the plan's preparation. Within the municipal government, this plan was prepared and approved by the Village of Hortonville Outdoor Leisure and Recreation Committee and adopted by the Village of Hortonville Board.



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## **CHAPTER 2**

### **GOALS AND OBJECTIVES**

## **CHAPTER 2: GOALS AND OBJECTIVES**

### **GOALS AND OBJECTIVES**

The following goals and objectives were developed to serve as a guide for the type of park and recreation system Hortonville residents would enjoy. The goals are stated as desirable conditions to strive for in the future. They are common ideals of the community that can be achieved through the actions of government leaders, private organizations, and individual citizens. Objectives specify in greater detail how these desired goals can be achieved. Together, the goals and objectives provide a basis for establishing Village policy regarding the planning, acquisition and future development of park and recreation facilities and build off what was presented in the 2035 Comprehensive Plan and Strategic Plan.

The following goals are developed from 7 categories:

1. Sufficient Parks and Recreational Facilities
2. Shorelines and Water Resources
3. Recreation Need
4. Multi-modal Facilities
5. Public Participation
6. Maintain Existing Facilities
7. Preserve and Protect Significant Natural Areas

### **GOAL 1**

To establish a community-wide system of parks, open space and recreation facilities that will provide all residents of Hortonville with adequate, convenient and high quality recreational opportunities both active and passive.

#### **Objectives**

- a. Ensure that all open space, parks and facilities planning is in full support of the Village's long range comprehensive plan and consider changes in land use.
- b. Further develop facilities at existing park sites to meet community park and recreation needs.
- c. Ensure that existing and newly developed residential areas are provided with adequate and accessible park and recreation facilities for all citizens.
- d. Where possible, emphasize the need for maintaining, upgrading and/or replacing deteriorating facilities over the development of new facilities.
- e. Provide safe and adequate accessibility to all Village park and recreation areas for pedestrians and bicyclists as well as motorists.

- f. Design active and passive recreational areas and facilities which can be utilized by elderly and handicapped citizens, i.e. ADA compliant.
- g. Promote cooperative efforts with surrounding communities to provide recreational facilities and programs as well as adding new facilities and programs.

## **GOAL 2**

Encourage and promote water-oriented recreation such as fishing, boating and swimming.

### **Objectives**

- a. Promote and endorse efforts to improve water quality, access, and navigability on Black Otter Lake.
- b. Take advantage of the opportunity to promote and maintain the boat launches that are in the Village.

## **GOAL 3**

To evaluate and analyze park, open space, and recreational services based on the changing needs of the Village and surrounding environs.

### **Objectives**

- a. Ensure that environmental and aesthetic qualities of the community are considered when planning for park and recreation development.
- b. Preserve and enhance other natural resource areas in and around the community.
- c. Provide periodic annual reviews of the plan to enhance the quality of life and promote community through parks and recreation.
- d. Complete the (Comprehensive Outdoor Recreation Plan) CORP to reflect the needs of the growing community.

*Comp Plan 2035 Strategy: Use the CORP to guide the development of new parks and trails. Strategy LU 1.7: Preserve and provide for open space and recreational areas to enhance the quality of the environment, maximize natural resource conservation, buffer incompatible uses and facilitate active and passive outdoor recreational pursuits by all age groups.*

- *Recommendation LU 1.7.1: Consider identifying locations for future parks, open space and public facilities.*
  - *Develop a town square.*
  - *Develop a village park in the downtown area.*

- *Identify a location for a future park on the Village's west side.*
- *Recommendation LU 1.7.2: Maintain and regularly update the Outdoor Recreation, Open Space, and Urban Forestry Plan.*

## **GOAL 4**

Provide multimodal facilities throughout the community.

### **Objectives**

- a. Encourage opportunities to improve, develop, and expand trails to parks and public facilities creating a network. Create better walkability to get to public parks to relax and recreate.

*(Comp Plan 2035 Recommendation IO 1.1.1: Consider sidewalk improvements, pedestrian and bicycle safety and accessibility to retail, community parks and recreational facilities, community facilities, Black Otter Lake, schools and residential neighborhoods when reviewing new development proposals or upgrading existing infrastructure.)*

- b. Create a way-finding system through the Village that would include a standard design for trailheads.
- c. Develop a "Complete Streets" policy on any new road projects where appropriate.

## **GOAL 5**

To encourage continued involvement of Hortonville residents when planning for park and recreation development.

### **Objectives**

- a. Conduct public meetings to provide opportunity for citizen involvement in developing the Village's park and recreation plan as well as a survey to gather public input.
- b. Encourage participation of all citizens at Village Board meetings.

## **GOAL 6**

Preserve and maintain all existing parks and public spaces

### **Objectives**

- a. Provide adequate resources and personnel to maintain current recreational facilities.
- b. Provide adequate accessibility to conform to ADA standards.
- c. Develop Master Site Plans for each park site to help guide future development.

## **GOAL 7**

Preserve and protect significant environmentally sensitive and historical areas.

### **Objectives**

- a. Identify and incorporate historical areas or structures in the development of Village parks,
- b. Encourage the use of natural features, such as floodplains, wetlands, and woodlands, as passive recreational areas.
- c. Protect significant wildlife areas.





## **CHAPTER 3**

# RECREATIONAL RESOURCES

## CHAPTER 3: RECREATIONAL RESOURCES

### RECREATIONAL RESOURCES AND FACILITIES

#### Current Conditions


There are 7 park sites within the Village of Hortonville's boundaries that range in size and service. Three of the areas are "traditional parks" and include: Alonzo, Black Otter Lake, and Miller, are Neighborhood Parks, Otto Miller Athletic Field is a Community Park and Mini-Parks: Veteran's Memorial and the Pocket Park located downtown. According to the state classifications listed below, the balance of parks and open space areas are unique and site specific, including boat launches and Black Otter Lake access. The High School provides an additional 11 acres of athletic fields along with private facilities like the Hortonville Commercial Club Park (14 acres) and Grandview Golf Course (64 acres).

A total of just over 30 acres of outdoor recreation land is currently available for public use that is maintained by the Village, (Map 1 and Table 1). A total of over 200 acres is available for residential use.

Hortonville does not exhibit a separate parks and recreation staff, instead they utilize the crew from the Public Works Department to help with the mowing and maintenance. Softball and baseball sporting events at the Otto Miller Sports Complex are organized by Village staff during the spring, with Hortonville Youth Sports scheduling Little League and other tournament events throughout the summer.

Determining if a community's open space needs are being met can often be a challenging task. The process depends heavily on input from community residents, officials, and staff. The best guide available for use in this plan includes standards from the National Recreation and Park Association. These standards represent minimum goals and are not applicable to all locations universally. It is also important to understand that each community has unique social, cultural, economic, and geographic factors that may limit the legitimacy of the standards being applied. See Chapter 4 of the needs analysis for detailed information and statistics.

Table 1: Park Matrix

| Hortonville Parks & Recreation Amenities  |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
|---|-----------------|-------------------|---------|-----------|---------|-------------|---------|----------------|----------|------|--------|----------------|-------------|------------|---------|-----------------|-------------|-----------|---------|---------|---------------|--------|----------|--------|-----------------|--------|------------|--|
|  | Ball Diamonds   | Basketball Courts | Benches | Boat Ramp | Camping | Concessions | Fishing | Floral Gardens | Football | Golf | Hockey | Horseshoe Pits | Ice Skating | Open Space | Parking | Playground Area | Picnic Area | Restrooms | Sandbox | Shelter | Sledding Hill | Soccer | Swimming | Tennis | Track and Field | Trails | Volleyball |  |
|   | Mini-Parks 1.14 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
|   |                 |                   | X       |           |         |             |         |                |          |      |        |                |             | X          | X       |                 |             |           |         | X       |               |        |          |        |                 | X      |            |  |
|   |                 |                   | X       |           |         |             |         |                |          |      |        |                |             | X          | X       |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
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|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
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|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
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|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
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|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
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|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |
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|   |                 |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |  |

| Park                                     | Acres | Ball Diamonds | Basketball Courts | Benches | Boat Ramp | Camping | Concessions | Fishing | Floral Gardens | Football | Golf | Hockey | Horseshoe Pits | Ice Skating | Open Space | Parking | Playground Area | Picnic Area | Restrooms | Sandbox | Shelter | Sledding Hill | Soccer | Swimming | Tennis | Track and Field | Trails | Volleyball |
|--|-------|---------------|-------------------|---------|-----------|---------|-------------|---------|----------------|----------|------|--------|----------------|-------------|------------|---------|-----------------|-------------|-----------|---------|---------|---------------|--------|----------|--------|-----------------|--------|------------|
| Middle School                            | 30    |               | X                 |         |           |         |             |         |                | X        |      |        |                |             |            |         |                 |             |           |         |         |               | X      |          |        | X               |        | X          |
| Elementary School                        |       | X             | X                 |         |           |         |             |         |                |          |      |        |                |             | X          | X       | X               |             |           |         |         |               |        |          |        |                 |        |            |
| Community Park 18.7                      |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Otto Miller Athletic Complex             | 18.7  | X             |                   | X       |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Large Urban Park                         |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| None                                     |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Natural Resource Areas 70.14             |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Wolf River Bottoms Preserve (T. Liberty) | 70.14 |               |                   |         |           |         |             | X       |                |          |      |        |                |             | X          |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Special Use Area Parks 1.63              |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Black Otter Fishing Pier                 | 0.2   |               |                   |         |           |         |             | X       |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Black Otter Park / Lake Access           | 0.3   |               |                   | X       | X         |         |             | X       |                |          |      |        |                |             |            |         |                 | X           |           |         |         | X             |        |          |        |                 |        |            |

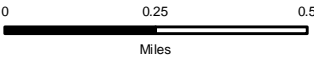
| Park   | Acres | Ball Diamonds | Basketball Courts | Benches | Boat Ramp | Camping | Concessions | Fishing | Floral Gardens | Football | Golf | Hockey | Horseshoe Pits | Ice Skating | Open Space | Parking | Playground Area | Picnic Area | Restrooms | Sandbox | Shelter | Sledding Hill | Soccer | Swimming | Tennis | Track and Field | Trails | Volleyball |
|--|-------|---------------|-------------------|---------|-----------|---------|-------------|---------|----------------|----------|------|--------|----------------|-------------|------------|---------|-----------------|-------------|-----------|---------|---------|---------------|--------|----------|--------|-----------------|--------|------------|
| Black Otter Shadows Boat Landing   |       |               | X                 | X       |           |         |             | X       |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Buchman Access (Wolf River, T Ellington – County Maintained) *   | 1.13  |               |                   | X       | X         |         |             | X       |                |          |      |        |                |             |            | X       |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Privately Owned Parks 78.66  |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Grandview Golf Course  | 64.38 |               |                   |         |           |         |             |         | X              |          | X    |        |                |             |            | X       |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Commercial Club Park - HCC   | 12.5  | X             | X                 |         |           |         | X           |         | X              |          |      |        |                |             |            |         | X               | X           | X         | X       | X       |               |        |          |        |                 |        |            |
| TOTALS: Village Park Acreage: 38.7 - excludes school prop, natural resource area, special use* , and private facilities. 200.74 acres for all listed.  |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Trails 19 Miles  |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Wiouwash State Recreational Trail - Trailhead at Lakeview Ave., 19 mile trail, 5.5 acres within Village, major pedestrian and bicycle trail, extends from Black Otter Lake to Oshkosh. Future plans call for extensions in phases to the north (Wolf River) and east (Fox Cities). |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |
| Hortonville Trail Connections – Through Alonzo & Black Otter Parks, along Black Otter Lake to downtown, Miller Park to S. Nash to Wiouwash   |       |               |                   |         |           |         |             |         |                |          |      |        |                |             |            |         |                 |             |           |         |         |               |        |          |        |                 |        |            |

# Map 1

## Village of Hortonville Parks and Recreation

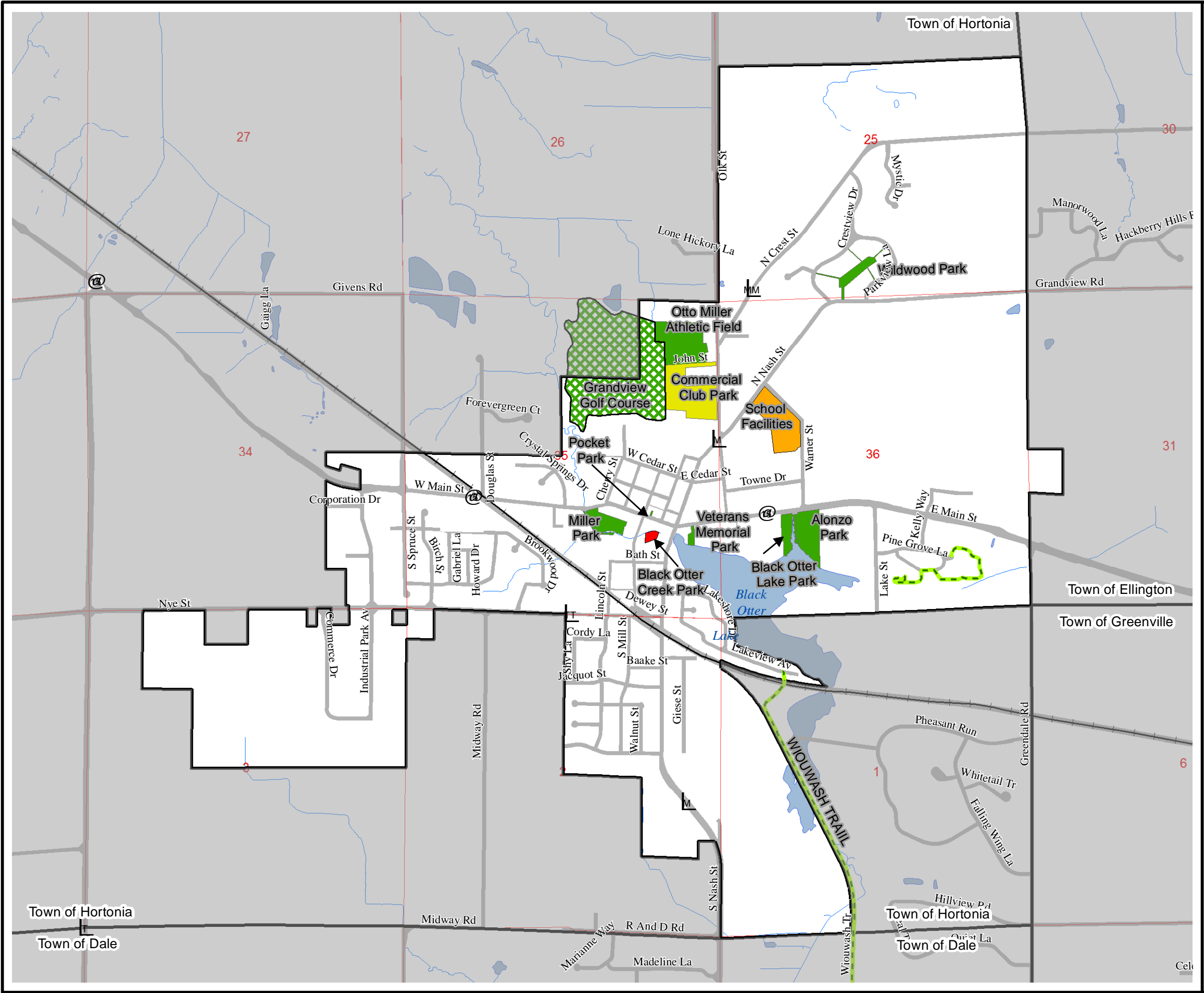
- Existing Trails
- Railroad
- Village Park
- Private Park
- School Facility
- Golf Course
- Planned Park
- Road
- Water

4



Source: Digital Base Data Provided by Outagamie County 2010  
Point Data Created by ECWRPC 2013

This data was created for use by the East Central Wisconsin Regional Planning Commission Geographic Information System. Any other use/application of this information is the responsibility of the user and such use/application is at their own risk. East Central Wisconsin Regional Planning Commission disclaims all liability regarding fitness of the information for any use other than for East Central Wisconsin Regional Planning Commission business.



## OUTDOOR RECREATION FACILITIES INVENTORY

### Mini-Park ¼ Mile Service Area

#### ***Black Otter Creek***

Undeveloped park next to the creek.

#### ***“Pocket Park”***

This is a .05 acre green space with a bench off Main Street.

#### ***Veteran’s Park***

Veteran’s Park is .4 acres in size and is located on Main Street, STH 15. This site serves as a memorial with a Gazebo, walking path, and historic marker. The adjacent lake corridor provides for future trail connections and park access.



### Neighborhood Parks ½ Mile Service Area

#### ***Alonzo Park***

Alonzo Park is an 11.5 acre park and is the welcome area into the Village. The park was developed with assistance from the Hortonville Lion’s Club which the shelter is named. It hosts playground equipment, picnic area, sand volleyball courts, restrooms and parking.

#### ***Miller Park***

Is located off of Main Street/STH 15, the park’s 4.8 acres contains a pavilion, unlighted softball diamond, tennis courts, picnic facilities, playground equipment, a sand box, court and new restroom facilities.

#### ***Wildwood Park***

This 3.1 acre park is located in the Wildwood Subdivision and is undeveloped. The area is rustic and natural green space.



## **Community Parks 1 Mile Service Area**

### ***Otto Miller Athletic Field***

Located west off of CTH M in the north part of the Village, this sports complex is 18.7 acres in size and hosts youth sports softball and baseball games. The site hosts 4 diamonds, bullpen, dugouts, restrooms, and 2 press boxes.

## **School Parks**

### ***Hortonville High School***

Hortonville High School is 25.7 acres in size and is located North of STH 15, on Warner Street. This site serves the students as well as the general public. This school park contains parking, play areas, basketball courts, ball diamonds, football fields, soccer fields, and a track.

### ***Hortonville Middle and Elementary School***

Hortonville Middle and Elementary School located across from the High School, the Middle and Elementary School complex offers 30 additional acres of open space, a playground, and soccer fields as well as basketball hoops.



## **Special Use Parks**

### ***Black Otter Launch***

Black Otter Launch is .21 acres in size located on the north shores of Black Otter Lake. This site has an unpaved area for parking car/trailer units and also a kayak launch site. During the winter months, this area is mainly used for snowmobiles and vehicles.

### ***Fishing Deck***

The fishing deck is located downtown, south of State Highway 15 along CTH M. There are stairs going down to the water and benches as well as a handicap accessible railing and ramp.





## **Trails**

### ***Local Trails***

There are .6 miles of local trails are located throughout the Village. Most are unpaved access connections. About 9.7 additional miles of trails are recommended or being planned through the Regional Bicycle and Pedestrian Plan.

### ***Wiouwash Trailhead***

The State Wiouwash trailhead is located off of Lakeview Avenue in the southwestern part of the Village. The County currently maintains the trail. There is a section of 19 miles that extends southward from the Village.



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## **CHAPTER 4**

# RECREATIONAL NEEDS

## CHAPTER 4: RECREATIONAL NEEDS

### RECREATIONAL NEEDS

East Central Wisconsin Regional Planning Commission Standards:

East Central Wisconsin Regional Planning Commission (ECWRPC), as part of its *Long-Range Transportation/Land Use Plan for the Fox Cities, Oshkosh, and Fond du Lac Urban Areas* (October 2010), has identified a level of service standard for parks, which is 10 acres for every 1,000 residents. ECWRPC created residential standards based on density for various services such as sanitary sewer, water supply, street network, and parks. The high density category is defined as three or more residential units per acre.

*As a policy, park and recreation programs should have a five-year cycle update for the Comprehensive Outdoor Recreation Plans (CORP). This will make the Village eligible to compete for Stewardship funding and other grants if so needed. (Appendix D)*

Park and recreation needs may include the demand for additional park land to accommodate new facilities, requirements for additional parks in areas where new residential growth is occurring, or the need for new or improved park facilities and equipment. Generally, recreation standards are used to identify these necessities. However, in communities the size of Hortonville, citizen input is equally useful when identifying park and recreation wants.

### POPULATION AND DEMOGRAPHICS

Approximately 2,711<sup>1</sup> people live in the Village that encompasses about 2,213 acres<sup>2</sup>. Between 2000 (2,357) and 2010 (2,711), the Village experienced modest population growth with its population growing by 15 percent during this time period. At the same time, the Village's land area increased by approximately 22.6 percent due to numerous annexations<sup>3</sup> (Table 2).

**Table 2: Population Trends and Projections, 1980-2040**  
**Village of Hortonville**

| Year  | Population | % Change |
|-------|------------|----------|
| 1980  | 2,016      | --       |
| 1990  | 2,029      | .64%     |
| 2000  | 2,357      | 16.16%   |
| 2010  | 2,711      | 15.02%   |
| 2015* | 2,735      | .88%     |
| 2020* | 2,895      | 5.85%    |
| 2025* | 3,030      | 4.66%    |
| 2030* | 3,150      | 3.96%    |

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<sup>1</sup> U.S. Census, 2010.

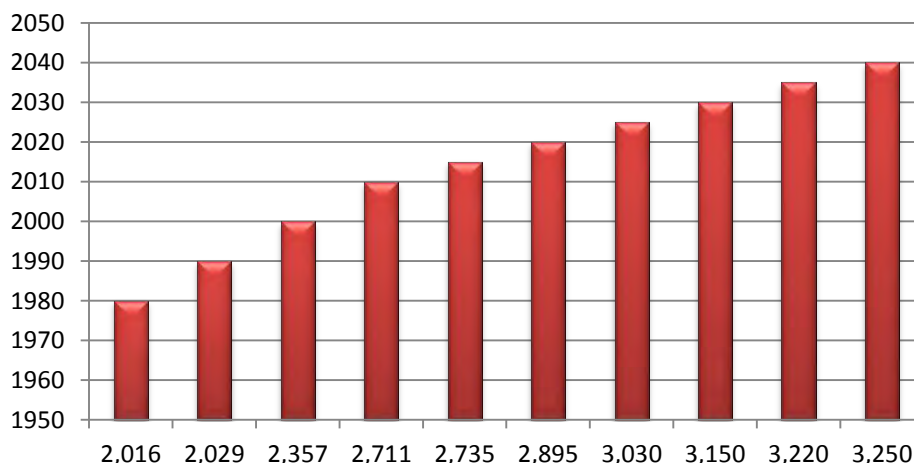
<sup>2</sup> East Central Wisconsin Regional Planning Commission, Land Use Inventory, 2013.

<sup>3</sup> U.S. Census, 408 acres annexed between 2001 and 2005.

|       |       |       |
|-------|-------|-------|
| 2035* | 3,220 | 2.22% |
| 2040* | 3,250 | .93%  |

Sources: Census 2010 Data, and  
 Department of Administration  
 projections 2013\*.

## Population by Year



## LAND NEEDS

Land demand standards of 10 acres of publicly owned open space for every 1,000 residents provides a useful barometer for determining whether a community has adequate parkland to meet the recreational needs of its residents. Based on this standard, Hortonville's park and open space acreage is currently adequate for its population base but in the future as growth continues the Village may run into a deficiency as it will be only 6.20 acres in surplus (Table 3).

Looking solely at the total recreational acreage currently owned by the Village and schools, there is not a definite need today for additional park land acquisition but in the foreseeable future as an increase in population pushes up the demand for major types of active recreational facilities, the acreage available will be cut in half by the year 2040. Maintaining and expanding the current parks system, should be one of Hortonville's priorities in accommodating its residents.

**Table 3: Hortonville Outdoor Recreation Demand, 2000-2040  
 Public Population Demand Projection Surplus**

| Year | Village Parks Acres | Population Projection | Demand Projection (10 acres/1,000 pop.) | Surplus / Deficiency (in acres) |
|------|---------------------|-----------------------|---|---------------------------------|
| 2000 | 38.7                | 2,357                 | 23.57                                   | 15.13                           |
| 2010 | 38.7                | 2,711                 | 27.11                                   | 11.59                           |

|      |      |       |       |       |
|------|------|-------|-------|-------|
| 2015 | 38.7 | 2,735 | 27.35 | 11.35 |
| 2020 | 38.7 | 2,895 | 28.95 | 9.75  |
| 2025 | 38.7 | 3,030 | 30.30 | 8.40  |
| 2035 | 38.7 | 3,150 | 31.50 | 7.20  |
| 2030 | 38.7 | 3,220 | 32.20 | 6.50  |
| 2040 | 38.7 | 3,250 | 32.50 | 6.20  |

## FACILITY NEEDS

The composition of a community's population can provide insight into the need for emphasizing specific types of recreational opportunities. Hortonville's population distribution by age group compares closely with that of Outagamie County (Table 4). Most of the residents living within the municipal boundaries are between the ages of **25-44 at 26.7% and 5-14 at 20.9%** which inhibits younger and middle age trends. Recreational facilities should be planned with this in mind to accommodate these age groups, i.e. playgrounds, sporting facilities, family picnic areas.

**Table 4: Population Distribution by Age Group (2009-2013)**

|              | Hortonville |             | Outagamie Co. |             | State of Wis.    |             |
|--------------|-------------|-------------|---------------|-------------|------------------|-------------|
| Age          | Number      | %           | Number        | %           | Number           | %           |
| 0-4          | 136         | 4.8         | 11,702        | 6.6         | 358,443          | 6.3         |
| 5-14         | 594         | 20.9        | 24,850        | 14.0        | 744,544          | 13.1        |
| <b>15-24</b> | <b>377</b>  | <b>13.3</b> | <b>23,215</b> | <b>13.2</b> | <b>785,761</b>   | <b>13.8</b> |
| <b>25-44</b> | <b>759</b>  | <b>26.7</b> | <b>48,111</b> | <b>27.3</b> | <b>1,447,360</b> | <b>25.4</b> |
| 45-54        | 397         | 14.0        | 28,090        | 15.9        | 873,753          | 15.4        |
| 55-64        | 279         | 9.8         | 19,893        | 11.2        | 699,811          | 12.3        |
| 65+          | 297         | 10.5        | 20,834        | 11.8        | 777,314          | 13.8        |
| TOTAL:       | 2,839*      | 100         | 176,695       | 100         | 5,686,986        | 100         |

\*Source: ACS Demographic and Housing 5 Year Estimates, 2009-2013

## JURISDICTIONAL STANDARDS

While immediate land needs for developed parks (10 acres per 1,000 residents) show Hortonville as a sufficient, there are also jurisdictional standards of what the State, County, the surrounding communities and private sectors are required to provide for in additional recreational opportunities. These are stated below:

- Recreational Lands Provided by the State----- 70 acres/1,000
- Recreational Lands Provided by the County----- 15 acres/1,000
- Recreational Lands Provided by the Local Community- 10 acres/1,000
- Recreational Lands Provided by the Private Sector----- 5 acres/1,000

Generally local communities should provide between 7 and 10.5 acres per 1,000 persons in developed park space as a standard rule of thumb. This varies when looking specifically at park type as shown in Table 5 for NRPA Specific Standards.

- The Village of Hortonville today (2010 Census, Table 3) provides a total of 11.59 acres of parks, recreation, and open space lands per 1,000 residents when utilizing current population. The surrounding school fields provide an additional 1.1 acres along with fishing access and special use parks adding another .16 acres bringing the total to 12.85. However, when utilizing the breakdown per park type, the Village is adequate in Neighborhood Parks but could use a few more Community or Mini-Parks to support usage.

**Table 5: NRPA Specific Standards**

| Park Type          | Acres per 1,000 residents | Town of Hortonville Acreage per 1,000 residents |
|--------------------|---------------------------|---|
| Community Parks    | 5 - 8 acres               | 1.87 acres                                      |
| Neighborhood Parks | 1 - 2 acres               | 1.94 acres                                      |
| Mini-Parks         | 0.25 – 0.5 acres          | .114 acres                                      |

NRPA = National Park and Recreation Association

### Regional Greenspace Comparison

Neighboring communities were researched for actual populations and green spaces. (Table 6) These parks and facilities also provide a level of service to Hortonville's population base but are usually located out of the ½ mile walking distance for residents.

**Table 6: Hortonville Comparison to Surrounding Communities for Outdoor Recreation Demand, 2010**

| Community                     | 2010 Population | Total Park Acreage | Needed Acres Per 1,000 Population<br>6.25-10.5A per 1,000 Capita |
|-------------------------------|-----------------|--------------------|--|
| City of Appleton              | 72,810          | 633                | 8.69   |
| City of Neenah                | 25,723          | 349                | 13.57  |
| Village of Little Chute       | 10,432          | 178                | 17.06  |
| City of Two Rivers            | 11,669          | 256                | 21.94  |
| <b>Village of Hortonville</b> | <b>2,711</b>    | <b>38.7</b>        | <b>11.59</b>   |
| Village of Kimberly           | 6,559           | 140                | 21.34  |

|                     |  |     |       |
|---------------------|--|-----|-------|
| City of Menasha     | 17,407                                 | 226 | 12.98 |
| City of Chilton     | 3,932                                  | 53  | 13.48 |
| City of Kaukauna    | 15,627                                 | 740 | 47.35 |
| City of Fond du Lac | 43,100                                 | 650 | 15.08 |
| Town of Menasha     | 18,545                                 | 283 | 15.26 |
| Village of Sherwood | 2,740                                  | 70  | 25.55 |
| Town of Grand Chute | 21,288                                 | 383 | 17.99 |
| City of Oshkosh     | 66,325                                 | 361 | 5.44  |
|                     |  |     |       |
| Regional Average    | <b>18.59 Acres per 1,000 residents</b> |     |       |

Other than the Cities of Appleton and Oshkosh, which fall below the acres of recreation needed to support their growing populations, Hortonville does lie below the regional average for available local acreage. Intergovernmental cooperation and non-profit partnerships could assist the Village on providing more opportunities within its boundaries as well as purchasing more land for the future.

## LOCATIONAL NEEDS

It is desirable that recreational opportunities be within convenient walking distance of each of a community's residents. With its compact development pattern and distribution of existing parks and schools, Hortonville residents walk to an existing recreation facility varies. A ½ mile is ideal as a walking distance but we need to also look at safety and use of sidewalks as well as bicycle paths. When residents are separated from the nearest park or school by natural barriers such as streams or man-made barriers such as busy streets, they are considered to be less adequately served because these features hinder a direct or safe passage. This is of particular concern for children and disabled or elderly residents. In Hortonville, the bulk of the neighborhood population exists in the northern tier of the Village. As new development encroaches upon agricultural land, available parks will need to be made available to serve those subdivisions as well as to support the current population growth in the area. Safe Routes School is a program that assists children in getting to school safely through either walking or bicycling. This is the same when looking at places to recreate.

## LEVEL OF SERVICE

The following classifications are derived from *the 2011-2016 Wisconsin Statewide Comprehensive Outdoor Recreation Plan – Appendix E*, and the *Park, Recreation, Open Space and Greenways Guidelines – A project of the National Recreation and Parks Association* and the



American Academy for Park and Recreation Administration. They have been modified to fit local conditions. See Map 2 for location of service areas.

**Table 7: Level of Service Classifications**

| Classification / Acres  | Description   | Location/Service Area   | Size             |
|---|---|---|------------------|
| <b>Mini Park</b> <ul style="list-style-type: none"> <li>Veteran's Memorial .40</li> <li>Black Otter Creek .69 (new undeveloped)</li> </ul>                        | A central space that provides recreation within a neighborhood, or as part of a larger neighborhood or community park and urban center, including retail shopping areas.                | Generally within a neighborhood of a <u>1/4 mile</u> radius or population of 2,000-3,000. Mini parks may be included in parks that serve a larger population or service area.                       | 0.5 – 1.5 acres  |
| <b>Neighborhood Park</b> <ul style="list-style-type: none"> <li>Alonzo Park (Lion's Club) 11.5</li> <li>Miller Park 4.8</li> <li>Wildwood (rustic) 3.1</li> </ul> | The basic unit of the park system. A neighborhood park, by size, program, and location, provides space and recreation activities for the immediate neighborhood in which it is located. | Serves a neighborhood within a <u>1/4 to 1/2 mile radius</u> , uninterrupted by non-residential roads and other physical barriers.  | 1 - 20 acres     |
| <b>Community Park</b> <ul style="list-style-type: none"> <li>Otto Miller Athletic Complex 18.7</li> </ul>   | Serves broader purpose than neighborhood park. Focus is on meeting community-based recreation needs, as well as preserving unique landscapes and open spaces.                           | Serves multiple neighborhoods within a <u>2 to 3 mile radius</u> , or when offering unique features, serves the entire Town. Located adjacent to collector streets providing easy vehicular access. | 20 acres or more |

Currently, the 2 Mini-Parks (Veteran's Memorial) service area boundary serves approximately 22% of the Village residents, 85% are served by the 3 Neighborhood Parks (Alonzo), 82% by the 1 Community Park (Otto Miller Sports Complex), and 14% of the population is not served by any parks within the municipal area. There are overlaps to the service areas and also serving the population outside of the municipal boundary into the Towns of Hortonville & Ellington. (Map 2)

**Table 8: Population by Park Type per Year**

| Park Type                        | 2010 | 2015 | 2019 |
|----------------------------------|------|------|------|
| Mini-Park                        | 600  | 616  | 637  |
| Neighborhood Park                | 2312 | 2399 | 2500 |
| Community Park                   | 2233 | 2311 | 2401 |
| All Parks                        | 2398 | 2486 | 2588 |
| Not Served, > 1 mile from a park | 378  | 397  | 418  |

The south side neighborhood along County Highway M currently is more than 1 mile from a park. Almost 400 residents are not being served by recreational uses. More park space should be considered near the Wiouwash State Trail.

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# Map 2 Village of Hortonville Parks and Recreation Level of Service

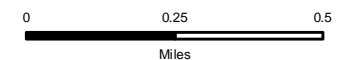
## Legend

- Existing Trails
- Village Parks, 39 Acres
- Private Park, 13.5 Acres
- School Facility, 11 Acres
- Golf Course, 64 Acres
- Planned Park, .7 Acres
- Mini Parks 1/4 mile service area
- Neighborhood Parks 1/2 mile service area
- Community Parks 1 mile service area
- Residential Areas
- Areas more than 1 mile to a park\*
- Railroad
- Parcels
- Water
- River/Stream
- Village of Hortonville

## \*Population served by Village Operated Parks:

| Park Type            | 2010 | 2015 | 2019 |
|----------------------|------|------|------|
| Mini-Park            | 600  | 616  | 637  |
| Neighborhood Park    | 2312 | 2399 | 2500 |
| Community Park       | 2233 | 2311 | 2401 |
| All Parks            | 2398 | 2486 | 2588 |
| Not Served, > 1 Mile | 378  | 397  | 418  |

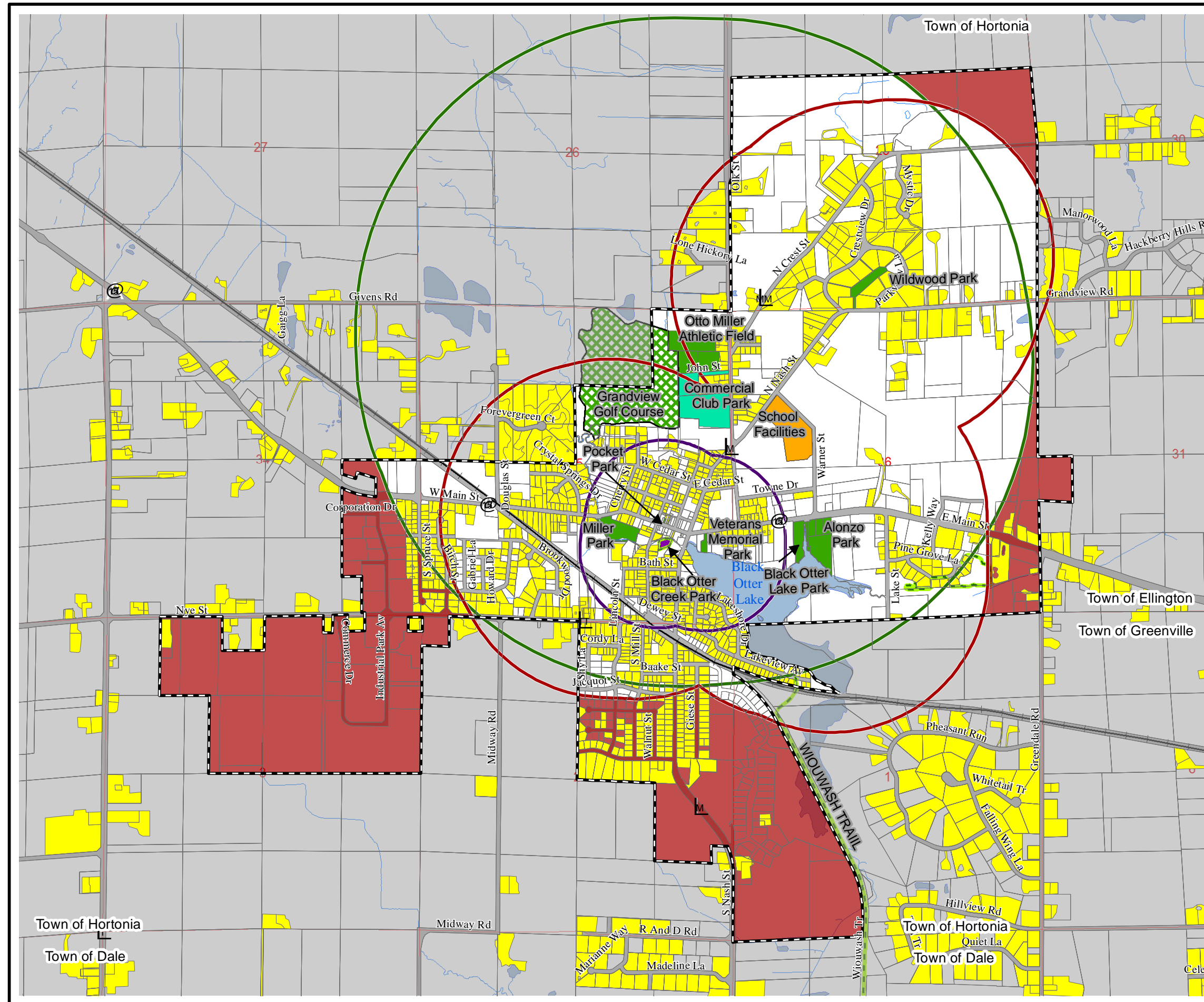
Please note: Service areas overlap and serve areas outside the Village's boundaries.



Source: Digital Base Data Provided by Outagamie County 2014.  
Point Data Created by ECWRPC 2014.

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Prepared March, 2015 by:

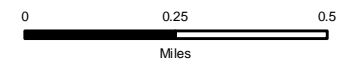


# Map 3 Village of Hortonville Population Density

## Legend

-  Existing Trails
-  Railroad
-  Residential Areas
-  Village Park
-  Private Park
-  School Facility
-  Golf Course
-  Planned Park
-  Water
-  Underserved Areas by Parks

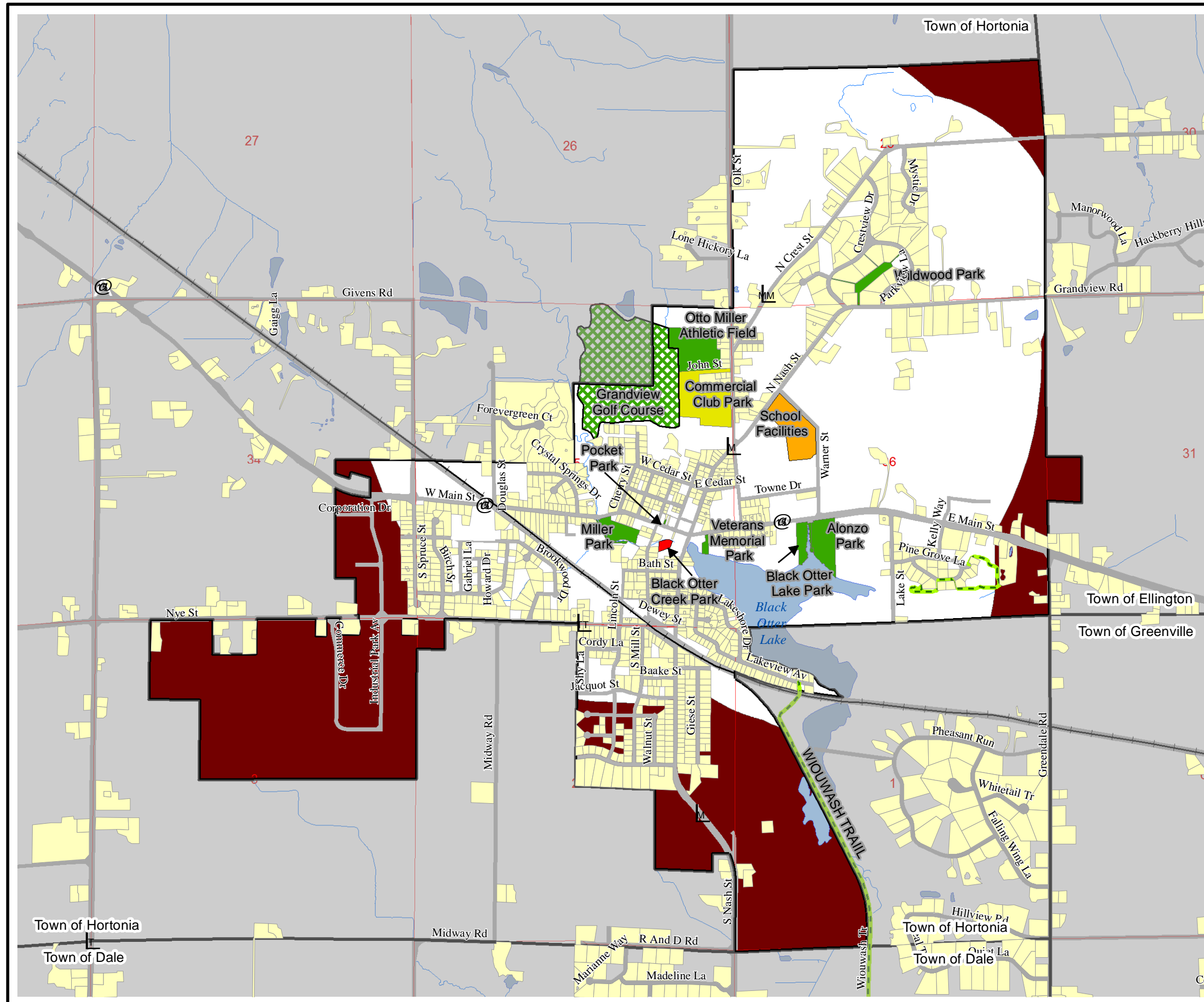
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Source: Digital Base Data Provided by Outagamie County 2010  
Parks and Landuse Data Created by ECWRPC 2015

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Prepared April, 2015 by:





## PUBLIC SURVEY SUMMARY AND ANALYSIS

A public survey was conducted in from fall to spring 2014-2015 and asked questions conducive to residential satisfaction, needs and wants, and overall facility use. Safety concerns in walking and biking were also included as well as questions for supporting more parks and trails. Below is a summary of the findings. For detailed results of the survey, please see Appendix A.

### Village of Hortonville – Survey Analysis:

#### INFO/DEMOGRAPHICS

- 1. Survey Participants Distribution:** *Most are within the Village's boundaries but a few respondents are traveling from New London and Medina to use the facilities.*



**2. Age Group:**

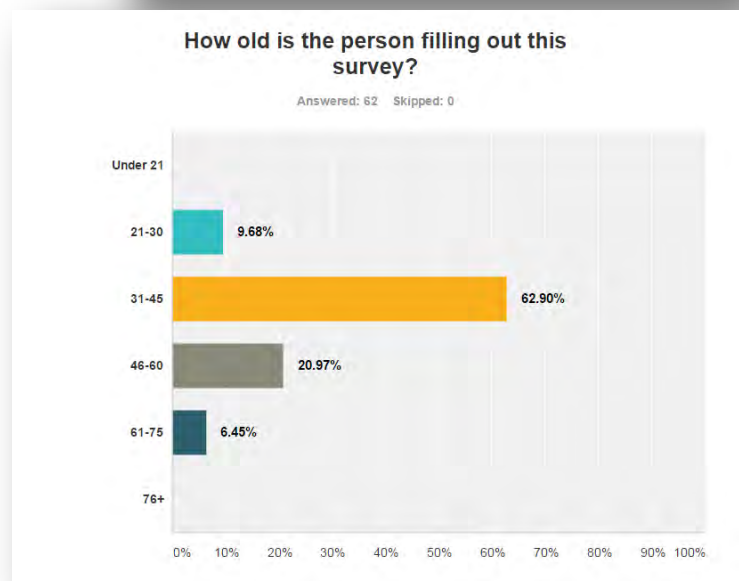
16-30

**31-45** 62.9%

46-60

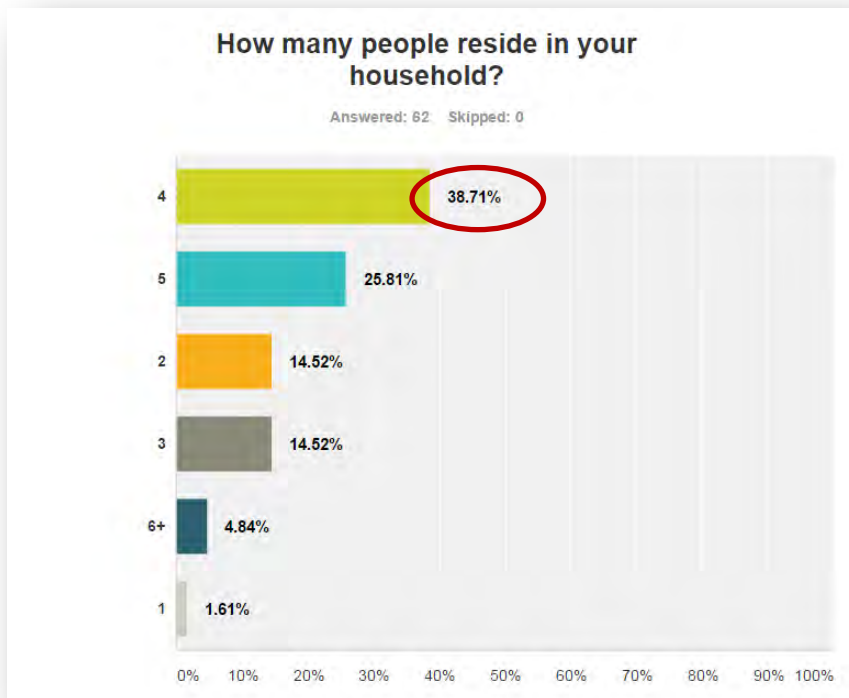
60-75

75+

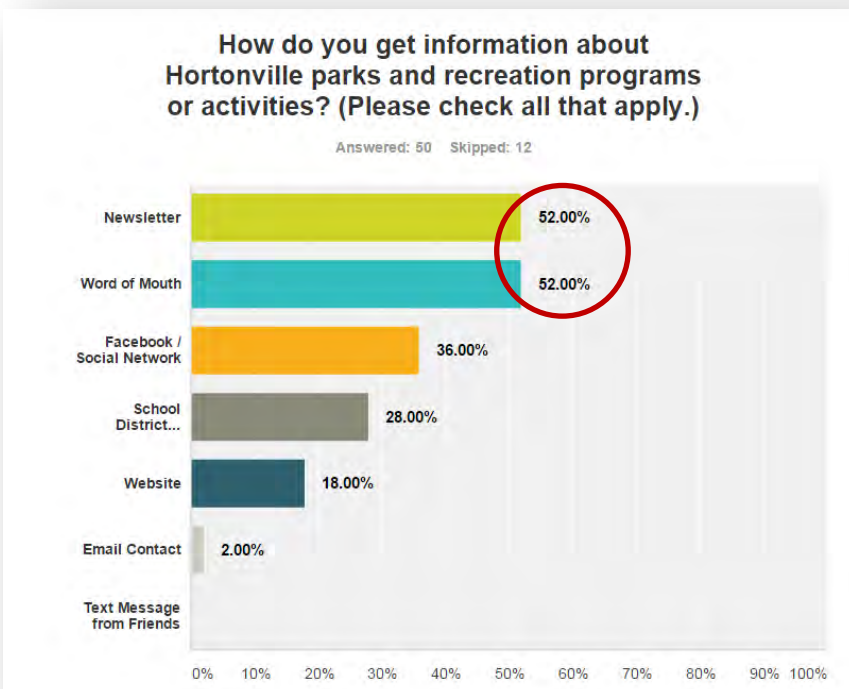


3. Are you Male or Female? 29.03% Male, **70.97% Female**

4. How many people reside in your household? Family of 4 was the #1 answer at **38.71%**



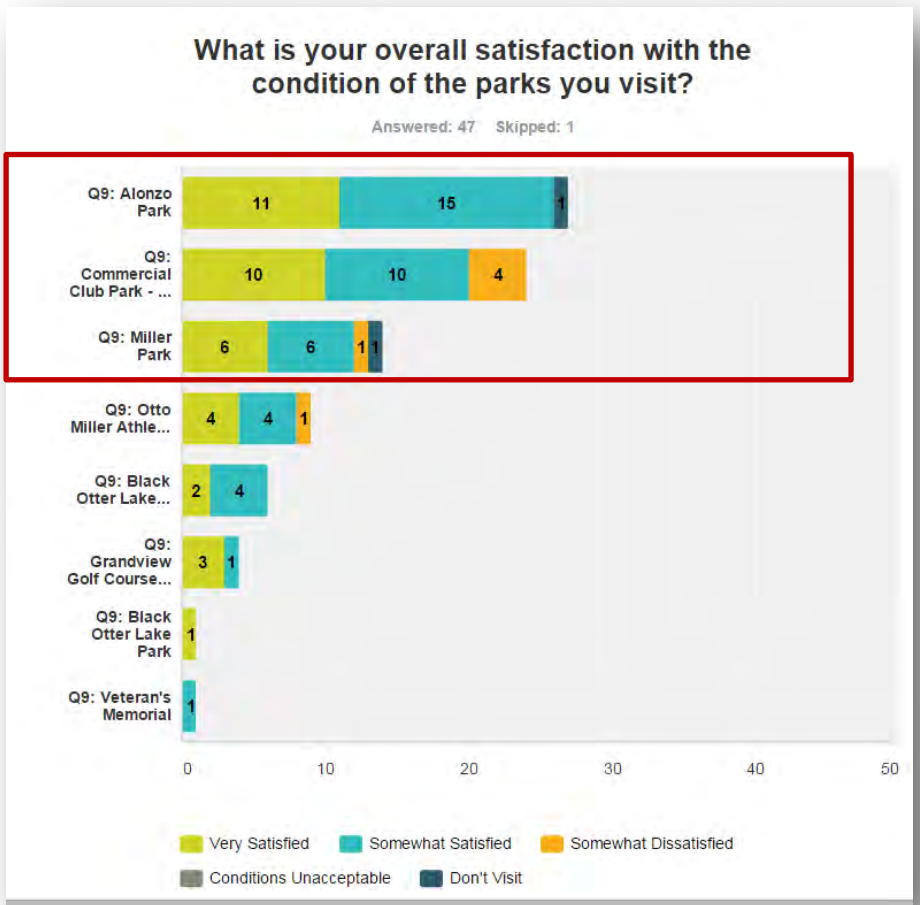
5. How did residents get their information? Newsletter and Word of Mouth tied for the #1 answer at 52%.



**SATISFACTION RATE:**

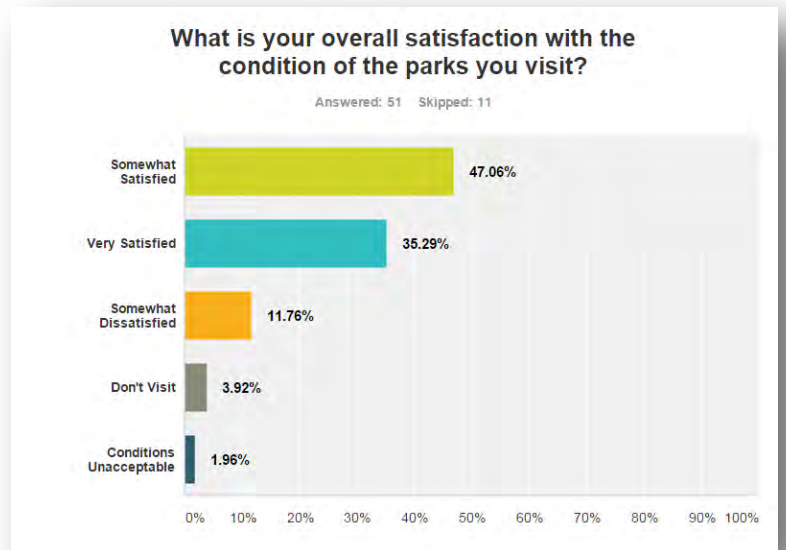
**1. All Parks and Facilities – Overall:**

Over 47% were somewhat satisfied with the parks and over 35% were very satisfied. Positive response rate by residents as many seemed pleased on what the Village is offering.



**2. Satisfaction by Park visited: Top 3 Favorite park/facilities were Alonzo, HCC, and Miller Park.**

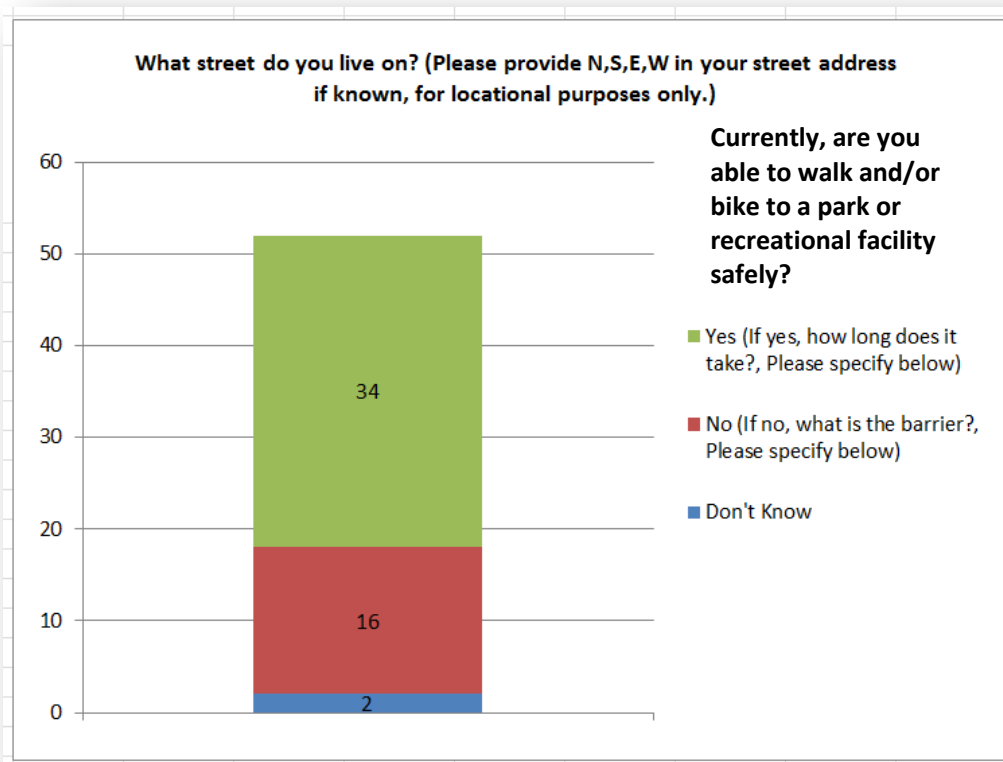
Of most of the parks visited, over 40% were very satisfied with the Black Otter Lake Fishing Pier, Hortonville Commercial Club, Miller Park, and Otto Miller Sports Complex. Most parks were considered acceptable.





**SAFETY AND ACCESS:**

**65.38% Yes, 30.77% No**



**Average Time**

10-15 minutes

**Street Barriers**

STH 15

Main Street

CTH M

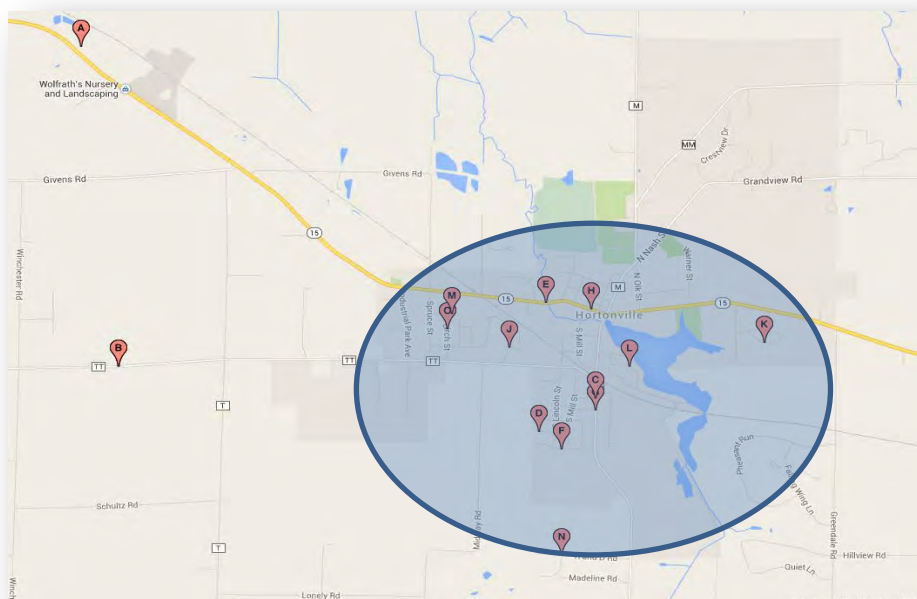
CTH TT

Lack of sidewalks to Jacquot or Honeysuckle at CTH M

Lake Street – need mowed path or sidewalk to park

Spruce Street no sidewalks

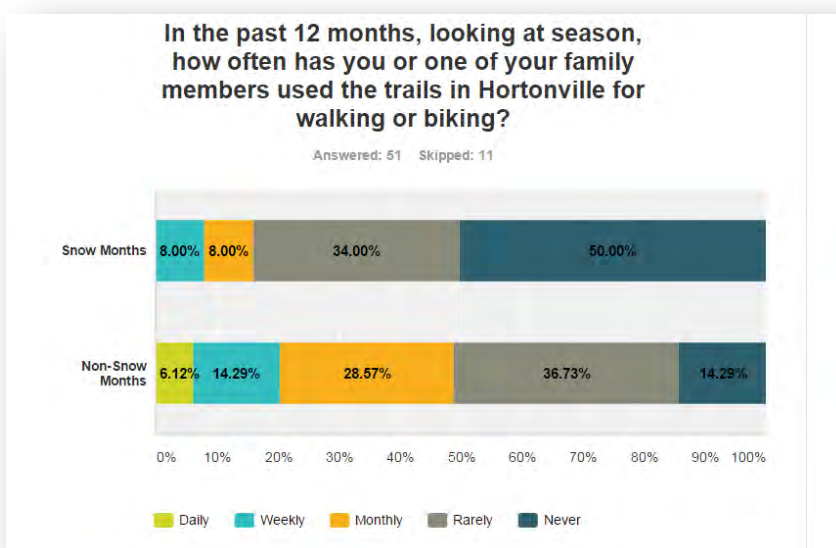
**Most residents who lived south of STH 15 were unable to get to Miller or Alonzo Park safely by biking or walking represented by the map below:**



## USE/MAINTENANCE FOCUS:

Monthly use during the non-snow months was the highest vs. rarely and never during the snow season.

Trail maintenance should be scheduled accordingly to accommodate usage patterns.



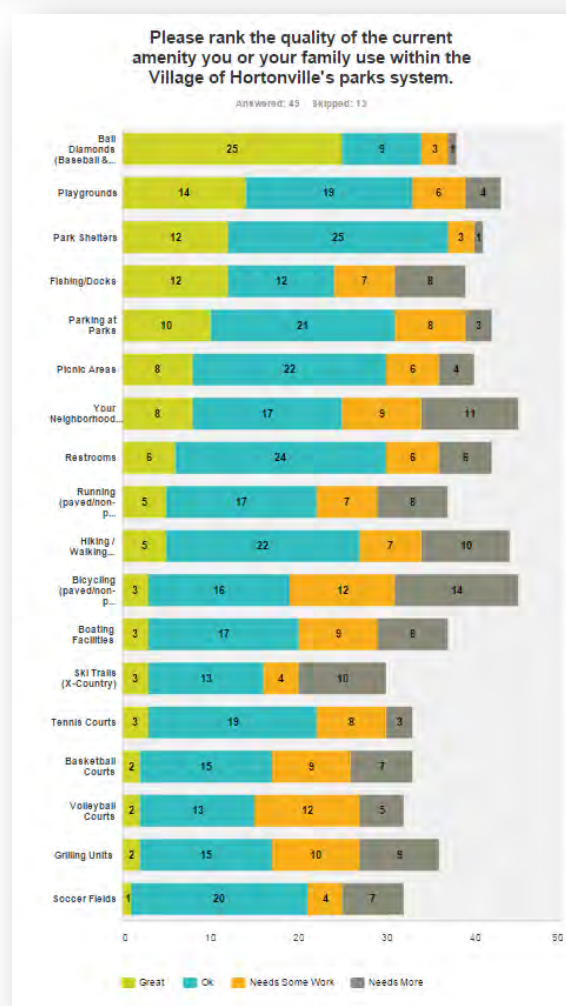
## QUALITY of the Amenity per USE:

### GREAT:

- *Ball Diamonds,*
- *Playgrounds,*
- *Park Shelters, and*
- *Fishing Docks were rated the best quality along with parking at the parks. (Shown in green)*

### NEEDS SOME WORK:

- Trails,
- Volleyball courts,
- Grilling Units, and
- Neighborhood Access needs some improvements. (Shown in orange.)



## NEED and PREFERENCES for TODAY

Residents were asked what facilities/programs are **needed now** in the Village.

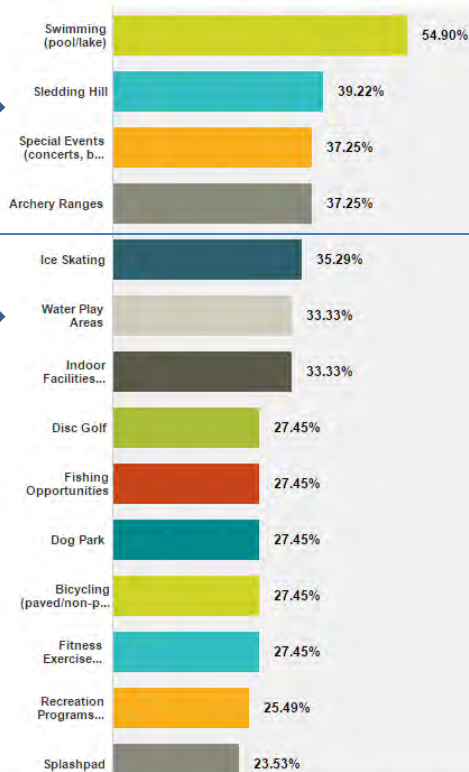
Popular answers: Pool, Sledding Hill, Special Events, and Archery Ranges.

Other responses included:

- Ice Skating Rink,
- Water Play Areas, and
- Indoor Gym Facilities.

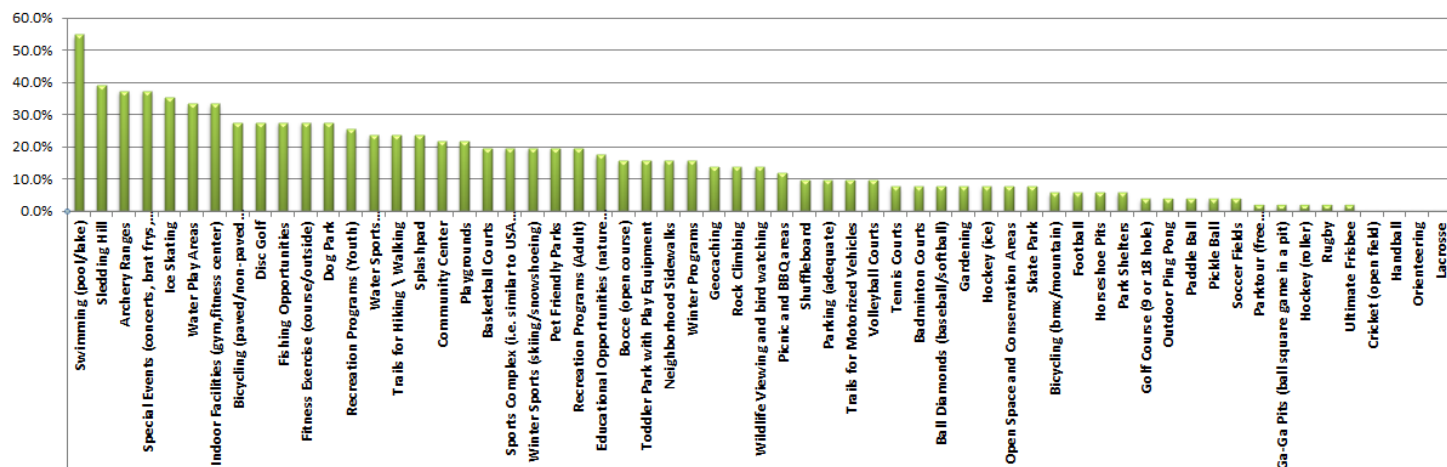
Please choose which activities you would like to see **ADDED** or **EXPANDED** to Hortonville's recreational system. (Mark all that apply.)

Answered: 51 Skipped: 11



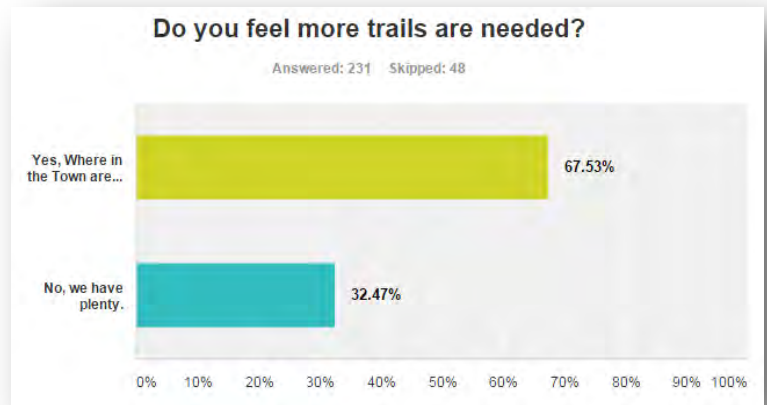
## Overall ratings of all activities:

Please choose which activities you would like to see **ADDED** or **EXPANDED** to Hortonville's recreational system. (Mark all that apply.)



### NEED and PREFERENCES for Today:

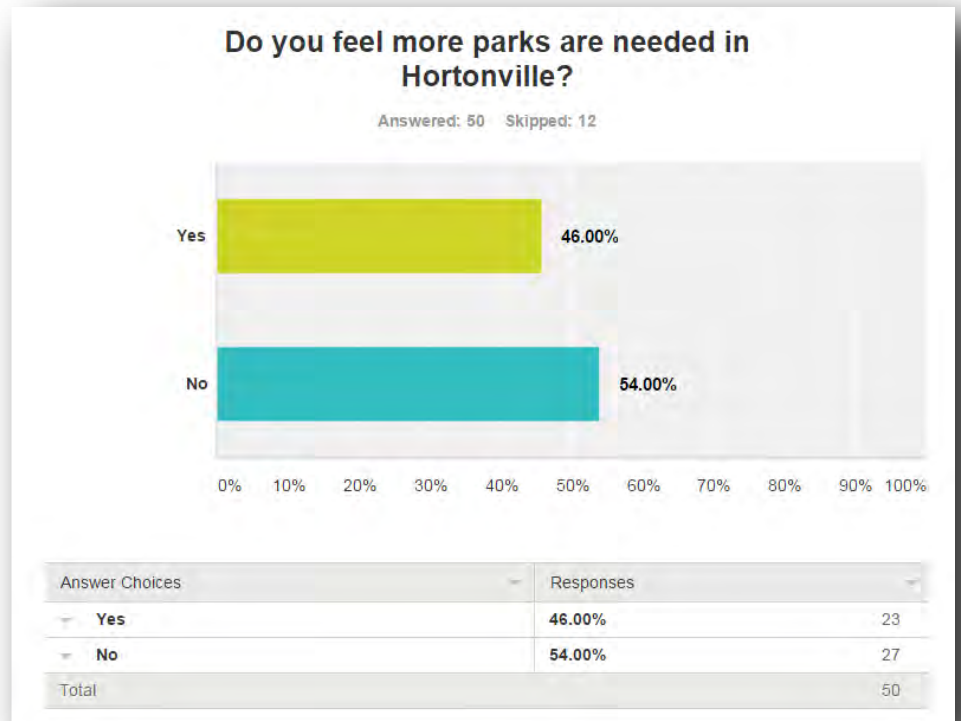
Over 70% stated they thought more trails were needed in the Village.



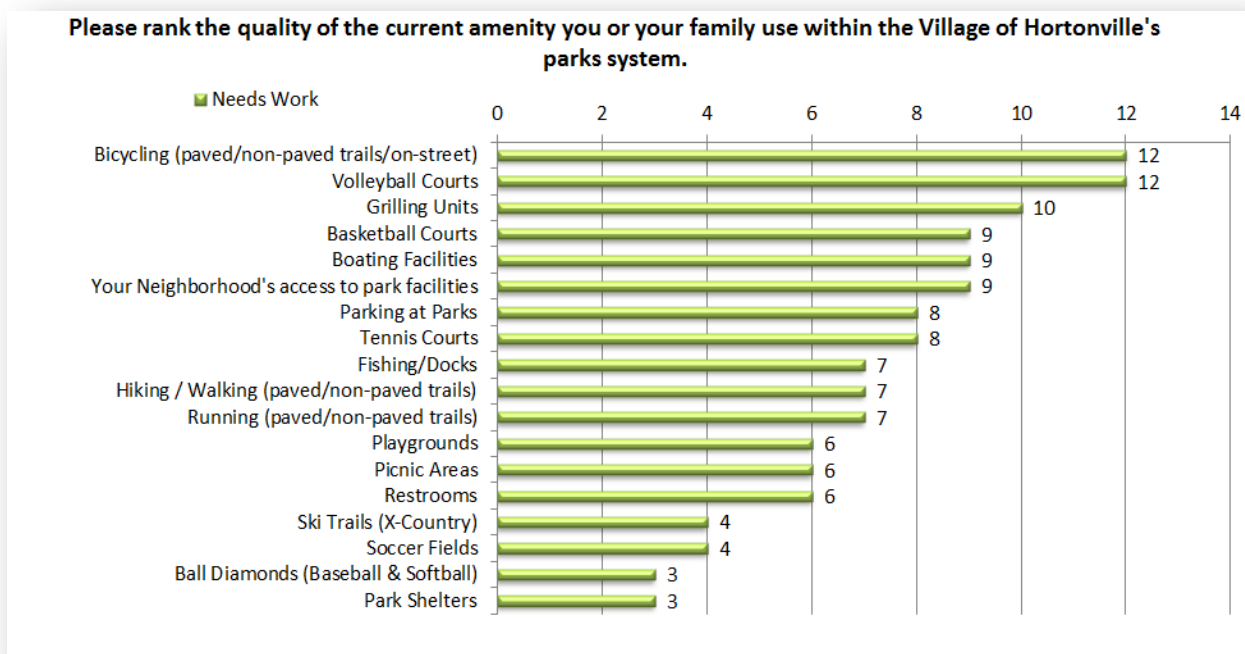
Possible Trail Connections: Trails linking parks to the downtown will be feasible once the STH 15 by pass is completed, 2018. Trails should be looked at when traffic improvements go in.

- WIOUWASH Trail to Alonzo and Black Otter Parks
- WIOUWASH Trail to the Downtown
- Alonzo and Black Otter Parks to the Industrial Park
- Greendale Road to Alonzo and Black Otter Parks
- Miller Park to S. Nash Street (Downtown)
- Veteran's Memorial Park to Fishing Pier (Downtown)

54% said there are plenty of parks in the Village but 46% thought more parks would be a nice addition.



## FACILITIES/PROGRAMS THAT NEED WORK IN THE VILLAGE OF HORTONVILLE:






**Trails** to the parks from a neighborhood were the number one answer followed by the **volleyball courts** and **grilling units**.

### Other comments:

In the comments section: Residents said to upkeep the current parks system should be a top priority. Creation of a pool or water play area was a popular comment, along with an ice skating rink and archery. Keeping dogs out of parks and more sidewalks for access were also popular.

**Table 9: Wisconsin Recreation Trends**

|   |                          |   |
|---|--------------------------|---|
| <b>Increasing Demand</b><br><br>   | Adventure racing         | Both as individual and group activity       |
|   | Driving for pleasure     | Easy activity for aging baby boomers        |
|   | Developed/RV camping     | Baby boomers continue to drive demand       |
|   | Kayaking                 | Participants attracted by cheap entry       |
|   | Visit a dog park         | Urban residents driving demand              |
|   | Soccer outdoors          | Urban youth driving demand                  |
|   | BMX biking               | X Games popularity may be driving force     |
|   | Climbing                 | Indoor climbing leading to outdoor climbing |
|   | Stand up paddling        | Popularity is sweeping the country          |
|   | Triathlon                | Varying distances allowed for growth        |
|   | Off-road vehicle driving | Post-recession growth continues             |
|   | Gardening/landscaping    | "Grow Local" concept taking hold            |
| <b>Stable Demand</b><br><br>      | Walk for pleasure        | Market saturation                           |
|   | Running or jogging       | Gen Y replacing baby boomers                |
|   | Water parks              | Recession caused growth to slow             |
|   | Motor boating            | Easy access to resources                    |
|   | Day hiking               | Popular with many generations               |
|   | Golf                     | Time constraints does not allow for growth  |
|   | Tent camping             | Stable, but growth is illusive              |
|   | Snowboarding             | May have peaked after 20 years of growth    |
|   | Trail running            | Stable niche with Gen Y                     |
|   | View wildlife            | Activity spans generations                  |
|   | Bicycle                  | Popular with easy access                    |
|   | Snowshoeing              | After large growth, this has stabilized     |
| <b>Decreasing Demand</b><br><br> | Hunting                  | Generational loss and private access        |
|   | Inline skating           | 6 years of decreasing participation         |
|   | Skateboard parks         | Youth are free-skating with longboards      |
|   | Horseback riding         | Recession impact caused decrease            |
|   | Softball                 | Baby boomers leaving the sport              |
|   | Downhill skiing          | Baby boom generation not being replaced     |

Source: 2011-2016 Wisconsin Statewide Comprehensive Outdoor Recreation Plan, Wisconsin Department of Natural Resources, August 2012.

State trends show an increase in camping, dog parks, kayaking, racing, pleasure driving, BMX biking, soccer, gardening, stand up paddle boarding, etc. Hortonville should consider the trends today and tomorrow when planning for additional recreational facilities. Multi-use and easily convertible accommodations would be the most cost effective.

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## **CHAPTER 5**

# HEALTH NEEDS ASSESSMENT



## CHAPTER 5: HEALTH NEEDS ASSESSMENT

### 2015 COUNTY HEALTH COMPONENTS AND STATISTICS

With obesity rates on the rise, Hortonville's need to promote active communities should be a top priority. From the 2015 Outagamie County Health Rankings\*, (with 1 being the best), Outagamie County is rated 23 overall out of 72 in the state for healthy outcomes (today) and 8 out of 72 for health factors (tomorrow), up from 2015 where 9 in health factors was charted but moved down 2 spots from 21 for overall healthy outcomes. When compared to the other counties in the state, these County Health Rankings illustrate what we know when it comes to what's making people sick or healthy, but understanding Hortonville through Outagamie County's rankings is only one component of improving the community's health. The "*Roadmaps to Health*" Action Center provides tools to help groups work together to create healthier places to live, learn, work and play. For example, In Winnebago County, the group re:Th!nk Winnebago is one such group who has started this process in working with the East Central Wisconsin Regional Planning Commission to promote health in planning.

Improving community health requires people from multiple fields to work collaboratively on an ongoing cycle of activities. Communities may be at different points in this process. Still within the recommendations for active living, ECWRPC has developed a regional bicycle and pedestrian plan to help guide this process. A plan of this nature will help fill in those voided connections residents are asking for while still planning for future active communities.



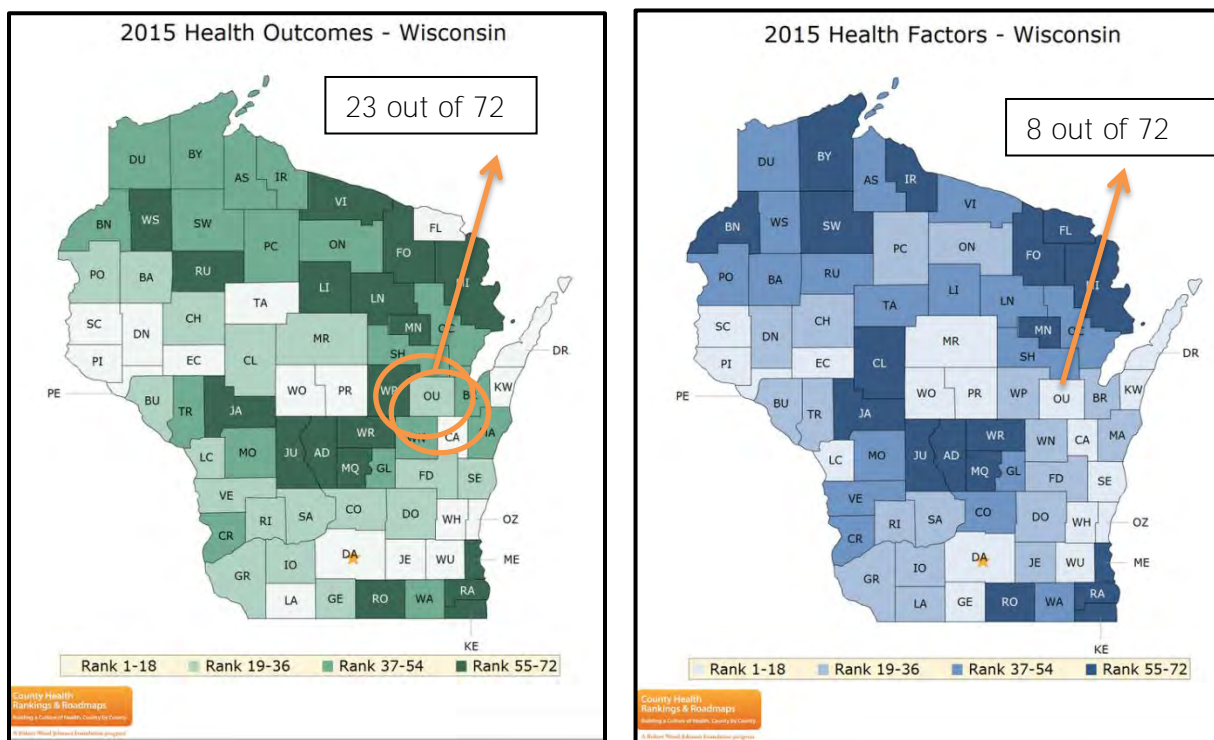
To enable and encourage the citizens of Hortonville to be physically active, a strengthening of the support and funding for the village's parks and recreation facilities as well as recreational programming should be looked at. "Park Prescriptions" should be written so to speak to encourage active use and enjoyment of what the Village has to offer. Improvements that would positively benefit public health and recreation are maintaining and adding more amenities to the existing Neighborhood and Community Parks to support its growing population base. Also adding more multi-use trail connections in general would help promote a healthy lifestyle and active living. Careful attention should be given to areas of people with lesser means so they have at least an equal level of opportunity for physical activity. This goes back to the Goal 1, Objective F of making the facilities ADA accessible in Chapter 2. In many large cities, low income neighborhoods can often lack quality recreational programs, playgrounds and parks. They can also be hindered in accessing nearby facilities by physical and land use barriers (i.e. transportation, water corridors and industry).

Continued support with community partnerships such as sport clubs, organizations, non-profits, citizen groups, companies, and other interested parties to develop, fund and operate recreation programs, leagues, and events are key. These partnerships are invaluable to help provide the opportunities for people to be physically active.

Parks, playgrounds, and open spaces provide opportunities for physical activity. However, the presence of facilities is not enough to make sure they are being used for the greatest benefit of

all. Factors such as location, accessibility, programming, connectivity, safety and aesthetics all play a role in the use of public facilities. By incorporating natural and design elements that have cultural or homeland meaning, will also help represent minority populations. Investing in consistent/uniform signage that aids people in identifying trails, places to be active, and other rec opportunities, should be considered. Promoting and marketing concepts should be developed along with the funding resources identified such as studies done through the Transform Wisconsin grant program.

Below are 2 maps that demonstrate where the County stands in the state as compared to the other 72 counties with 1 being the healthiest. Factors such as smoking, obesity rates, drinking excessively, STD's and motor vehicle crashes make up the first map,



while outcomes such as premature death, poor to fair health, poor physical days, low birth weight, and poor mental days are shown on the second map. The rankings comparing the rest of the state are as follows:

Multi-modal transportation such as walking, biking, and public transportation can also help enhance the health and overall physical activity of all residents, by improving air quality and the environment, and reducing overall traffic congestion. Numerous Hortonville residents choose to walk and/or bike as a primary means of transportation due to cost of vehicle ownership and maintenance, environmental choice, healthy lifestyle choice, etc. These types of initiatives are recognized at the state, national, and international-level to create safe, convenient, affordable, and fun opportunities for residents to walk, bike, and/or utilize public transportation as a primary means of transportation. Health and safety should be a priority concern to the citizens of Hortonville. Safe Routes to School programs can assist communities and encourage children to walk and bike to school as long as connections are in place and barriers are crossed.

Lastly, according to a publication from the Trust for Public Land, there are 8 ways that parks can improve your overall health:

1. Parks increase exercise. People who live closer to parks are shown to exercise more as the opportunity is there and people who regularly use parks get more exercise than people who don't.
2. Most moderate exercise takes place in parks, approximately 12%. Parks are where people get the kind of exercise their bodies need most.
3. Parks reduce stress and clear the mind. Outdoor areas boost focus and promote relaxation as the term being one with nature implies.
4. A park makeover with aesthetically pleasing spaces encourages use and promotes public health habits.
5. A variety of amenities can attract different users, i.e. skateparks, basketball and tennis courts, baseball, paths, and therefore encourage fun activities.
6. Dedicated fitness zone exercise areas encourage exercise and increase park use. The Village of Menasha just put in workout benches at Fritsch Park where parents can get in reps while watching their kids play on the playground. Body fitness stations are becoming popular to encourage adults to workout.
7. Supervised activities and programming maximize a park's public health benefit. Getting people out and moving in a fun active setting is the best medicine.
8. Exercise is one of the cheapest ways to stay healthy. By promoting healthy habits, parks can help reduce health care costs.

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## **CHAPTER 6**

# RECOMMENDATIONS

## CHAPTER 6: RECOMMENDATIONS

Hortonville currently has adequate land for future recreational use but should look at acquiring more to accommodate its growing population in the next 5 -10 years. Most recreational facilities are in short supply or are simply not available in all portions of the community. In addition, many existing facilities are getting over used and will need renovation or refurbishing if they are to continue providing quality recreational opportunities. To more accurately assess the needs as they relate to the Village of Hortonville, the Outdoor Leisure and Parks Committee conducted a thorough analysis of its parks and recreational facilities. The needs they identified focused on specific recreational facilities currently not available, in limited supply, or in demand of additional improvement or expansion. They determined that adding or upgrading these facilities will help achieve the Village's goal of providing residents with a wide variety of recreational opportunities.

The proposals in this plan are intended to serve as a guide for the future development of park and recreation areas in the Village. Although some of the recommendations may not be feasible in the near future, they represent courses of action that should be taken if the opportunity or need arises. Projects that are feasible for implementation within the next few years are included in the community's five-year action program. These should be reviewed annually by the Parks Committee to accommodate changing needs of the community.

### PARK RECOMMENDATIONS FOR EXISTING PARK SITES BY TYPE

#### 3 Mini-Parks

##### ***Pocket Park – Downtown sitting area on Main Street***

- Turn area over to a commercial or retail use.
- Consider creating the former old library site into a picnic area or downtown square as a new mini-park.

##### ***Veteran's Memorial Park***

- Install a fishing pier/dock. (CIP 2016)
- Maintain Gazebo – paint needed, relocate closer to the lake for a nice picnic area.
- Upkeep the Kiosk by the Lake.
- Construct a pedestrian trail to the fishing pier on S. Nash St. and to Alonzo/Black Otter Parks. (CIP 2017/2019)

##### ***Black Otter Creek Park – New Proposed***

- Develop a master plan for the park.
- Add signage.
- Create a small picnic area by the creek.

## **4 Neighborhood Parks**

### **Alonzo Park**

- Improve drainage issues by the pavilion.
- Add a preschool (2-5 aged) play structure.
- Add trail to Greendale Road. (CIP 2017)
- Comply to ADA standards – handicap signage / parking.
- Add in bike racks – could be a high school project.

### **Black Otter Lake Park**

- Add a handicap kayak/canoe launch platform. (“Lake District”)
- Pave drive and in designated boat / trailer parking.
- Add in benches facing lake through donations.
- Control weeds and aquatic invasive species on the lake. (“Lake District”)
- Add landscaping around Kiosk area.
- Add a picnic area.
- Consider a security dusk to dawn light or “dummy” camera.

### **Miller Park**

- Install modern swing set / play apparatus (6-12 aged) and surfacing for the playground, look into donations for structure. (2015 CIP in progress)
- Construct a half basketball court. (CIP 2018)
- Construct a trail from Black Otter Dam to the park. (CIP 2017)
- Construct a splash pad. (CIP 2016)
- Pave parking area. (CIP 2019)
- Consider adding in an ice skating rink, could be a shallow swimming / wading area also in the summer if feasible.
- Add picnic tables near the creek.

### **Wildwood Park**

- Explore and research current contract. Consider selling to adjacent property owners extending their back yard space if feasible or give back to the donated family.

## **1 Community Park**

### **Otto Miller Sports Complex**

- Pave concession area. (CIP 2016)
- Expand and improve parking lot area. (CIP 2017)
- Consider refurbishing the old soccer field area after the STH 15 bypass is completed.
- Continue with improvements such as new lighting upgrades, etc.

### General New Park Projects

- Consider a site for a pool or swimming lake. (CIP 2020 long range)
- New neighborhood park (3-5 acres) – Nye Street Area. (CIP 2018)
- New community park (20-25 acres). (CIP 2020 long range)
- Pedestrian bridge – Black Otter Creek Park to downtown after STH 15 bypass is completed. (CIP 2020 long range) (New parking lot)
- MSC Nature Trail educational markers. (2015 CIP in progress)
- Signage throughout system – way-finding. (2015 CIP in progress)
- Add more tennis courts. (CIP 2018)
- Add a special event to the park's programming (top 3 in request on survey) – Village Square / Farmer's Market area. If the parcel would come available, purchase land across from gas station/bowling alley for the farmers market and extra parking.
- Consider a dog park.
- Hire a program coordinator to help with organizing the youth programs and events.
- Consider adding in a sledding hill with a warming area.
- Update all trash receptacles for all parks.

### BICYCLE AND PEDESTRIAN RECOMMENDATIONS

On September 6, 2013, the Village, with the assistance from ECWRPC developed a “*Trail Planning and Connections Report*”. The report considered local levels of service for trail provision, recommended areas of first priority for development and acquisition, and improvements to existing facilities. The recommendations in this plan follow the report's concept plans and also include some of the recommendations on connections from the ECWRPC Regional Bicycle and Pedestrian Plan should be used as a guide for detailed trails development and implementation.

Multi-modal accommodations in a community must serve various user groups, such as bicyclists, in-line skaters, wheelchair users, and pedestrians. This makes it necessary to look at the corridors from a variety of user points of view.

The recommendations in this plan are based on several widely recognized publications.

The main publications used include the following:

- *Americans with Disabilities Act Accessibility Guidelines (ADAAG)*
- *Guide for the Development of Bicycle Facilities*, The American Association of State Highway and Transportation Officials (AASHTO)
- *Pedestrian Facilities Users Guide*, U.S. Department of Transportation, Federal Highway Administration
- *The Manual on Uniform Traffic Control Devices (MUTCD)*
- *Wisconsin Bicycle Planning Guidance*, Guidelines for Metropolitan Planning Organizations and Communities in Planning and Developing Bicycle Facilities, Wisconsin Translinks 21



- *Wisconsin Pedestrian Planning Guidance*, Guidelines for Metropolitan Planning Organizations and Communities in Planning and Developing Bicycle Facilities, Wisconsin Translinks 21

## Definitions

### ***Pedestrian Facilities***

Walkways are areas set aside for people traveling on foot. The best walkways are those that will make people feel safe, have good access, are aesthetically pleasing and attract pedestrians.

1. **Sidewalks or Walkway:** Sidewalks and walkways are “Pedestrian Lanes” that provide people with space to travel within the public right-of-way that is separated from roadway vehicles.
2. **Curb Ramps:** Curb ramps or Wheelchair ramps provide access between the sidewalk and roadway for people using wheelchairs, strollers, walkers, crutches, handcars, bicycles, and also for pedestrians with mobility impairments who have trouble stepping up and down high curbs.
3. **Marked Crosswalks and Enhancements:** Marked Crosswalks indicate optimal or preferred locations for pedestrians to cross as well as show areas where vehicles must yield to pedestrians.
4. **Roadway Lighting Improvements:** Quality lighting and adequate placement can add dramatically to the comfort and safety of pedestrians. Without sufficient lighting motorists may not be able to see the pedestrians in adjacent walkways.
5. **Street Furniture and Walking Environments:** Sidewalks should be a continuous system that has access to various goods, services, transit, and homes. Well developed and well used walking environments are enhanced by the use of street furniture such as benches, shelters, trash receptacles, and water fountains.

### ***Bicycling Routes***

The goal of a bicycle route system is to provide bicyclists with good mobility and access within the major transportation corridors. Mobility is the ability to move from one place to another in the most direct route and with the fewest interruptions. Access is the ability to get where you want to go once you have reached the general area of your destination.

To meet these goals, various criteria have been used to determine route placement and what facilities will be used on these routes. Three general approaches are usually used to place routes:

1. Space bicycle routes ¼ to ½ mile apart throughout the urbanized area. This method has the advantage of consistency but does not lend itself well to areas that have a large number of barriers to transportation like lakes, rivers, hills, or freeways. Transportation barriers force a deviation in the grid and may leave key areas unserved (Wisconsin Translinks 21, 19-20).
2. Ensure that important trip generators and destinations are connected by bicycle routes. This has the advantage of serving the most bicyclists, but again it may leave some areas unserved.

3. Attempt to provide all arterial streets with wide curb lanes or bicycle lanes. This approach addresses the observation that bicyclists tend to have the same desired destinations as motorists and tend to use the same transportation corridors. However, because of right-of-way and other limitations this may not always be possible. Any new street should follow the "Complete Streets" policy which the Village should consider adopting.

Other factors to be considered include safety (actual and perceived); cost, and how well the route system will integrate with other modes of transportation.

Because of the unique characteristics of the Village of Hortonville, a combined approach which employs components from all of the above approaches should be used to best serve bicyclists.

### **Bicycle Facilities**

Once you have decided to place a bicycle facility within a corridor, the type of bicycle facility to be used must be selected. The types of facilities typically used are listed below:

1. **Route Signs:** All routes, regardless of facility type, should be marked with route signs. This makes it easier for bicyclists to use the system. Routes should also be given names which describe the route (i.e. University Route).
2. **Shared Roadway:** Many residential streets are perfectly acceptable for bicycling without any further improvements. On these streets, route signs are used to indicate a preferred route of travel for bicyclists.
3. **Paved Shoulders:** Paved shoulders are typically used on roads without curb and gutter. They provide bicyclists with a smooth surface outside of the main travel portion of the road on which to operate. Roads with paved shoulders are also much safer for motor vehicle drivers in that these roads experience far fewer head-on collisions.

4. **Wide Curb Lanes:** Wide curb lanes are the minimum treatment for arterial streets. Wide curb lanes allow bicyclists and motorists to share a travel lane without adversely affecting each other. On streets without parking, wide curb lanes are typically 14-15 feet wide. This does not include the curb and gutter section. Wide curb lanes also benefit motor vehicle traffic. In fact, wide curb

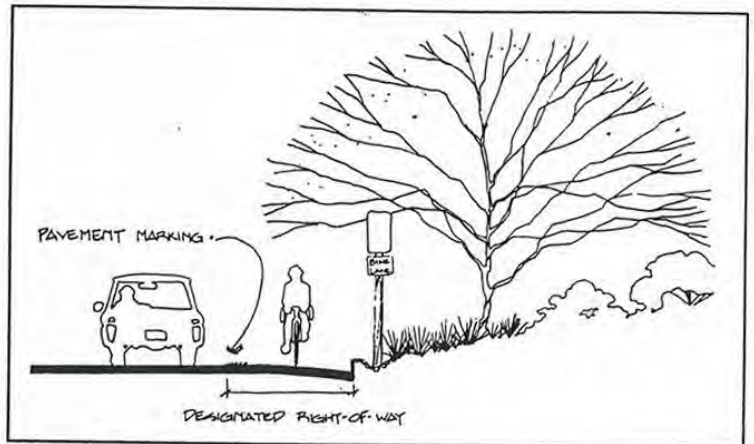


Figure 1 Bicycle lane (Harris et al, 341-3).

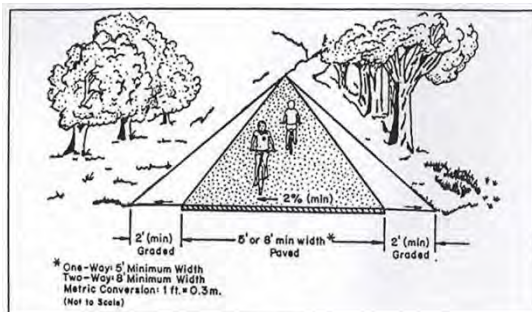


Figure 3 Recommended widths for bicycle paths (AASHTO, 24)

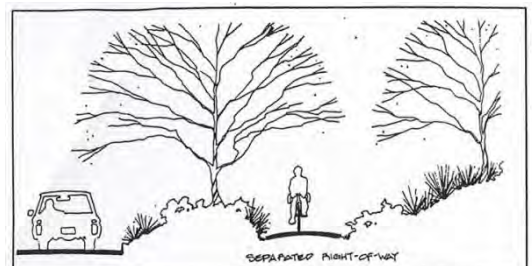


Figure 2 Bicycle Path (Harris et al, 341-3)

lanes were originally designed to improve motor vehicle traffic flow.

5. **Bicycle Lanes:** Bicycle lanes are a portion of the roadway which has been designated by striping, signing, and pavement markings for the preferential or exclusive use of bicyclists. They are usually 4-6 feet wide. Bicycle lanes are perceived by many bicyclists as being safer and thus encourage bicycling on these facilities. (See figure 2.)

6. **Bicycle Paths:** Bicycle paths are a bicycle facility separated from the roadway by some sort of barrier or space (see figure 2). The recommended minimum widths for bicycle paths are 5-foot for a one-way path and 8-foot for a two-way path. (See figure 3). If the path is to be used by pedestrians as well as bicyclists, the minimum recommended path width is 10'. (See figure 4.)

### Multi-Modal Recommendations

The following is a list of potential multi-modal corridors. These areas have been chosen based on guidelines described in this plan and the Village's Trail Planning and Connections Report (See Appendices).

#### Corridors – Traveling North to South

- Wiouwash Trail to Alonzo and Black Otter Parks (CIP 2016)
- Wiouwash Trail to the Downtown

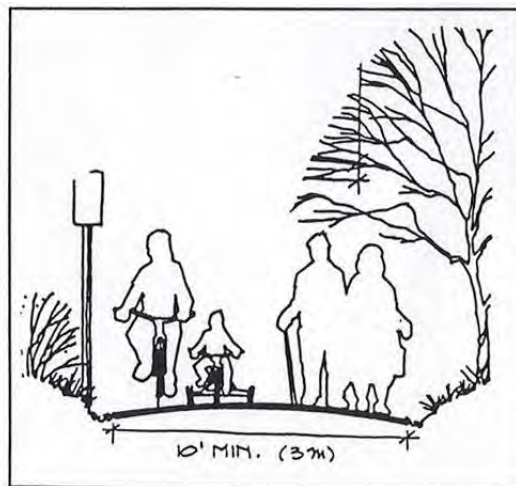


Figure 4 Recommended minimum multi-use path width (Harris et al, 341-6)

#### Corridors – Traveling East to West

- Alonzo and Black Otter Parks to Industrial Park
- Greendale Road To Alonzo and Black Otter Parks (STH 15 trail) (CIP 2017)
- Miller Park to S. Nash Street
- Veteran's Park to Fishing Pier S. Nash Street to Downtown (CIP 2017)
- Veteran's Park to Fishing Pier S. Nash Street to Alonzo Park (CIP 2019)
- Black Otter Dam to Miller Park (CIP 2017)
- Need general sidewalks in newer subdivisions for safety concerns. Develop an ordinance and work with developers to include this like stormwater ponds are for flooding concerns. A sidewalk along Nye Street and S. Nash Street near the Black Otter Supper Club would be beneficial. Consider a Complete Streets Policy or initiate a Safe Routes to School program.



Wiouwash State Trailhead

The selected corridor routes provide connections to daily trip generators (i.e., parks, schools, business district). Review of specific routes for perceived and actual safety concerns need to be studied on a case by case basis. Upon design of these facilities, final determination of the specific "type" of accommodation shall be established by the Village of Hortonville Board.

## **Accessibility Guidelines**

*Accessible* describes a site, building, facility, or portion thereof that complies with the Americans with Disabilities Act Accessibility Guidelines (ADAAG) as interpreted by the Department of Commerce in the Wisconsin Administrative Code and Register for Barrier-Free Design.

Newly designed or newly constructed and altered recreation facilities shall comply with the applicable requirements for accessibility to buildings and facilities by individuals with disabilities under the Americans with Disabilities Act (ADA) of 1990.

## **SPECIAL ISSUES AND PROGRAMS**

### **Urban Forestry**

The Village of Hortonville should continue toward developing an Urban Forestry program, and also strive toward increasing the Tree City USA years of membership. Clean air and water, aesthetics and community image are greatly increased by urban forestry.

### **Village and School Cooperation**

School Districts are important entities in the community's open and recreation space. Combined planning is instrumental to secure adequate facilities which are maintained well for use of the entire community. Shared planning efforts may assist in developing the number of active recreation spaces, maintenance responsibilities and costs, as well as cooperation of use and operations of all facilities such as with the Hortonville High School and Youth Programs.

The Village should continue to work with the area school district personnel and staffs to continue this positive cooperation since the population are utilizing the school grounds. Open communication and utilizing all available land and resources will continue to provide the community positive results for active recreational lands.

### **Hortonville and Private Sector Cooperation**

Due to current funding constraints on municipal governments throughout the State of Wisconsin, the private sector and user groups are beginning to assist in park development and ongoing site maintenance. User groups have established relationships with Hortonville in assisting the maintenance of baseball fields and soccer fields. The Village needs to continue this open communication as well as developing on-going agreements which allow user groups to assist in funding or providing "in-kind" funding for the development and maintenance of the fields and areas they are directly using.

### **Shared-Use Parks**

Park development, improvement and maintenance are costly and especially difficult with shrinking budgets. This plan incorporates several joint park developments with neighboring communities. Parks should and do serve everyone and not just the individuals that live within your community. The development of parks with neighboring communities will spread these

costs out to two or more communities and provide a park that serves residents in adjacent communities.

### ***Maintenance***

Inspection of The Village of Hortonville park and recreation facilities indicated that Village facilities are well-maintained. Well-maintained park areas not only protect public investment, but insure safety. Park staff should continue to provide services, which adequately operate and maintain park and recreation areas and facilities. To ensure continued excellence in park maintenance, the following policies should continue to be implemented:


- Provide routine maintenance and cleanliness for all facilities.
- Provide for non-recurring maintenance needs, repairs and replacements.
- Annually inspect all facilities for safety and maintenance needs.
- Appropriate funds to meet the needs for proper maintenance.
- Maintain facilities to meet the standards of the Americans with Disabilities Act (ADA).
- Develop athletic field maintenance programs to meet the states NR151 Regulations.
- Encourage parks staff to attend training conferences to continuing education opportunities.
- Set standards for mowing heights and frequency.
- Set standards for infield maintenance, “lip” reduction, etc.
- Set standards for removal of a field for use based on safety concerns and field conditions.
- Develop a list of when events should be cancelled and who makes the call. (Program Coordinator)
- Complete and document monthly playground inspections.

### ***Non-Capital Projects***

- Consider a tree nursery.
- Add in a community garden.
- Market the parks and what the Village has.
- Add a Senior Center and more programming.



# Map 4 Village of Hortonville Park & Trail Recommendations

- Proposed Trail Segment
- Existing Trails
-  Railroad
- Proposed New Park\*
- Village Park
- Private Park
- School Facility
- Golf Course
- Planned Park
- Water

\*General locations for new parks, not purchased property. Recommendations only.

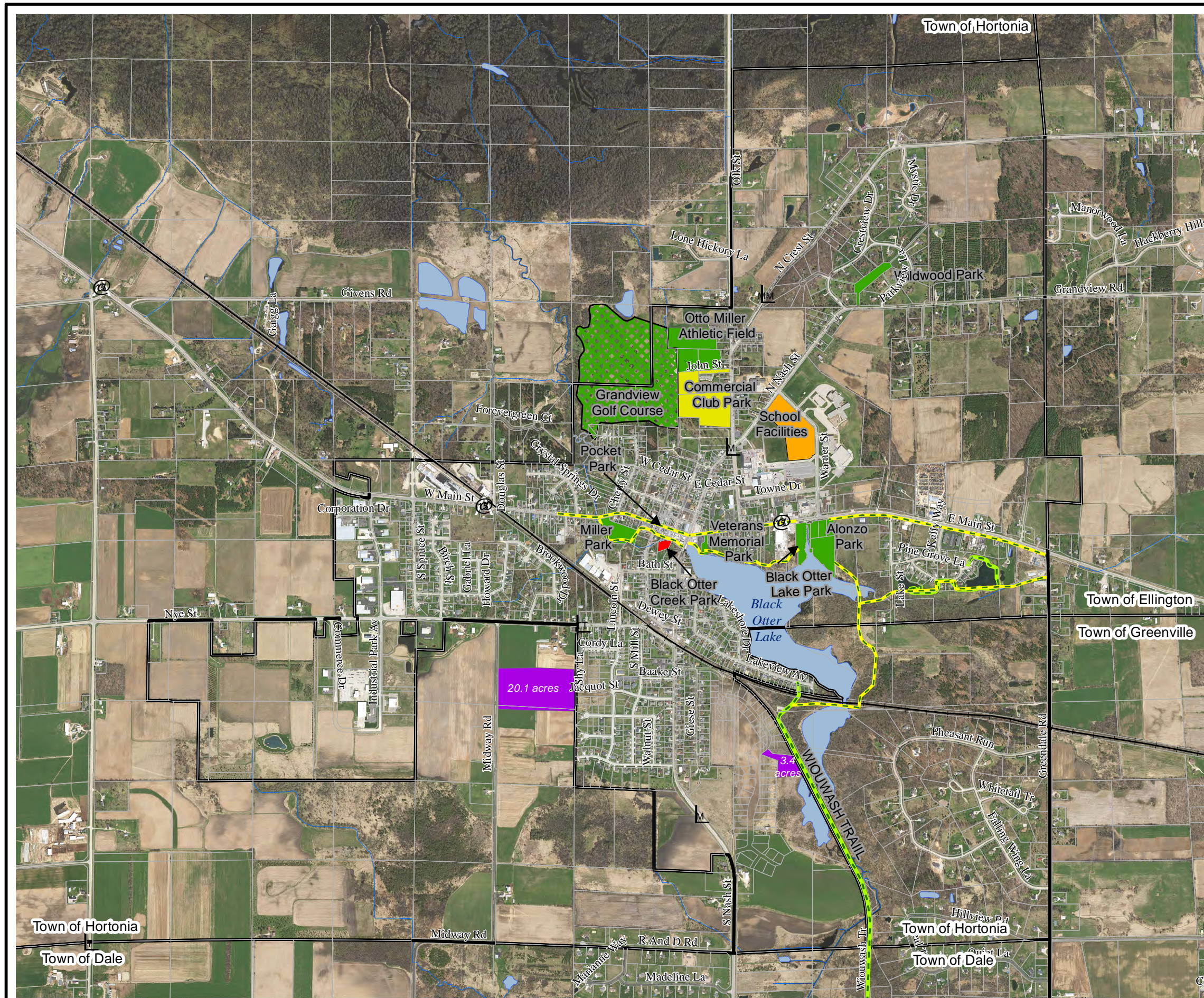
## 4

0 0.25 0.5  
Miles

Source: Digital Base Data Provided by Outagamie County 2010  
Parks and Landuse Data Created by ECWRPC 2015

This data was created for use by the East Central Wisconsin Regional Planning Commission Geographic Information System. Any other use/application of this information is the responsibility of the user and such use/application is at their own risk. East Central Wisconsin Regional Planning Commission disclaims all liability regarding fitness of the information for any use other than for East Central Wisconsin Regional Planning Commission business.

Prepared April, 2015 by:







## **CHAPTER 7**

### **ACTION PROGRAM**

## **CHAPTER 7: ACTION PROGRAM**

### **5 YEAR ACTION PROGRAM, 2015-2019**

The projects listed below, Table 11, comprises the action program set forth by the Hortonville Outdoor Leisure and Parks Committee for implementation during the next 5 years and beyond. This action program is based on priority level by year to serve as a guide in planning for future facilities. These were formulated to see where the community should be expending its resources from 2015-2019. This will also assist service clubs and organizations to select potential park improvement projects. As updates and development nears, more costs should be researched to assist in project budgeting and be included within the estimated Capital Improvement Program.

Many of the projects identified in the action program are potential candidates for receiving cost sharing monies available through the WDNR, (Appendix D) while others could be developed through the efforts and/or monetary support of volunteers and community organizations.

This plan works to develop and manage a working system of park and recreational facilities for the Village of Hortonville. It has been prepared to respond to expressed desires its residents and to conform to the comprehensive plan as accepted standards for provision of facilities. It provides the basic information and direction for future actions of the Parks Committee, and the Village Board. Much work remains to implement the proposals.

### **CAPITAL IMPROVEMENTS**

The following section provides a plan for implementing the recommendations over the next five-year period. Cost estimates are provided to assist the Village in preparing for funding.

Estimates are based on 2014-2015 costs and do not reflect potential future price increases. The estimates provided are for the total cost of the project, and do not take into account any potential funding assistance, even though some projects may qualify for grant funding but usually a match is required. This document is a five year plan, but depending on population growth and future development the plan could guide the Village for many years beyond 2019 which is noted in the 2020 long term category.

#### **Project Priority**

In order to provide a concise action plan for the recommended projects, a priority year for each project was established with input from Hortonville's Public Works Department. The park based projects are listed in the following Capital Improvements Schedule in the order of their priority by year. Ultimately the priority and the dollar amounts spent on each project will be decided by the Village Board on a project by project basis for budget approval.



**Table 10: 5-Year Action Plan and Priority List**

**Needed Now Projects Underway 2015-16, Short-Term 2017, Long Term 2018-19+**

**Listed in terms of priority: High-Highest (4-5), High (4), Average-High (3-4), Average (3), Low-Average (2-3), Low (2), Lowest-Low (1-2), Lowest (1)**

**CAPITAL IMPROVEMENT COSTS: (major park renovations)**

| <b>Priority Level</b> | <b>2015</b>                |   |                       |               |
|-----------------------|----------------------------|---|-----------------------|---------------|
|                       | <b>Park / Amenity</b>      | <b>Description</b>                                    | <b>Estimated Cost</b> | <b>Totals</b> |
| 4                     | Environmental Education    | Develop MSC nature trail with educational markers.    | \$6,000               | \$6,000       |
| 4                     | Signage                    | Implement Federal Guidelines Per Year                 | \$3,500               | \$3,500       |
| 4                     | Black Otter Lake           | Spillway Replacement Design and Construct             | \$600,000             | \$600,000     |
| 4                     | Otto Miller Athletic Field | New Storage Shed                                      | \$7,500               | \$7,500       |
| 4                     | Miller Park                | Install Modern Swing Set and Surfacing                | \$8,000               | \$8,000       |
| 4                     | Alonzo Park                | Restroom Remodel                                      | \$6,000               | \$6,000       |
| 3-4                   | Trails                     | Expand bicycle and hiking trails                      | \$20,000              | \$20,000      |
|                       |                            |   | <b>Total</b>          | \$651,000     |
| <b>Priority Level</b> | <b>2016</b>                | <b>Description</b>                                    | <b>Estimated Cost</b> | <b>Totals</b> |
| 4                     | Signage                    | Implement Federal Guidelines Per Year                 | \$3,500               | \$3,500       |
| 4                     | Veteran's Park             | Install Fishing Pier / Dock                           | \$15,000              | \$15,000      |
| 3-4                   | Otto Miller Athletic Field | Concession Area Pavement                              | \$20,000              | \$20,000      |
| 2                     | Miller Park                | Construct a Splash Pad                                | \$40,000              | \$40,000      |
| 3-4                   | Trails                     | Expand bicycle and hiking trails, Per Year Allocation | \$20,000              | \$20,000      |
| 3-4                   | Wiouwash Trail             | Extend to Alonzo Park – Engineering costs             | \$8,000               | \$8,000       |
|                       |                            |   | <b>Total</b>          | \$106,500     |

| Priority Level | 2017   | Description   | Estimated Cost | Totals    |
|----------------|--|---|----------------|-----------|
| 3              | Trails*<br><br>*Could be done in phases per the By-Pass Construction and Complete Streets assignments. | Main St. construct trail from Alonzo Park to Greendale Rd. (Hwy 15 trail)       | \$243,500      | \$943,500 |
|                |  | Main St. at-grade bicycle/pedestrian lanes/trail from Alonzo Park to Spruce St. | \$300,000      |           |
|                |  | Pedestrian trail from Veteran's Park to the fishing pier on S. Nash St.         | \$150,000      |           |
|                |  | Pedestrian trail from Black Otter Lake dam to Miller Park                       | \$250,000      |           |
| 2-3            | Otto Miller Athletic Field   | Expand/Improve Parking lot  | \$28,000       | \$28,000  |
| 3-4            | Trails   | Expand bicycle and hiking trails (per year allocation)                          | \$20,000       | \$20,000  |
| 4              | Signage  | Implement Federal Guidelines Per Year   | \$3,500        | \$3,500   |
|                |  |   | <b>Total</b>   | \$995,000 |
| Priority Level | 2018   | Description   | Estimated Cost | Totals    |
| 1-2            | Pedestrian bridge  | Across Black Otter Creek from new parking lot (above) to downtown               | \$250,000      | \$250,000 |
| 2-3            | Tennis Courts  | Location to TBD   | \$70,000       | \$70,000  |
| 3-4            | Miller Park  | Construct a half Basketball Court   | \$15,000       | \$15,000  |
| 2              | Land Acquisition   | Neighborhood Park 25 acres. Nye Street Area                                     | \$162,500      | \$162,500 |
| 3-4            | Trails   | Expand bicycle and hiking trails (per year allocation)                          | \$20,000       | \$20,000  |
| 4              | Signage  | Implement Federal Guidelines Per Year   | \$3,500        | \$3,500   |
|                |  |   | <b>Total</b>   | \$521,000 |
|                |  |   |                |           |

| <b>Priority Level</b> | <b>2019</b>             | <b>Description</b>                                     | <b>Estimated Cost</b> | <b>Totals</b>    |
|-----------------------|-------------------------|--|-----------------------|------------------|
| 1                     | Trail                   | From fishing pier (or Veteran's Park) to Alonzo Park   | \$60,800              | \$60,800         |
| 1                     | Miller Park             | Pave Parking Area                                      | \$34,500              | \$34,500         |
| 3-4                   | Trails                  | Expand bicycle and hiking trails (per year allocation) | \$20,000              | \$20,000         |
| 4                     | Signage                 | Implement Federal Guidelines Per Year                  | \$3,500               | \$3,500          |
|                       |                         |  | <b>Total</b>          | <b>\$118,800</b> |
| <b>Priority Level</b> | <b>2020+ Long Range</b> | <b>Description</b>                                     | <b>Estimated Cost</b> | <b>Totals</b>    |
| 1-2                   | Swimming Area           | Wading Pool / Shallow Swim area Installation           | \$100,000             | \$100,000        |
| 1                     | Land Acquisition        | Community Park 29 acres                                | \$188,500             | \$188,500        |
| 3-4                   | Trails                  | Expand bicycle and hiking trails (per year allocation) | \$20,000              | \$20,000         |
| 4                     | Signage                 | Implement Federal Guidelines Per Year                  | \$3,500               | \$3,500          |
|                       |                         |  | <b>Total</b>          | <b>\$312,000</b> |

| <b>Summary Totals</b>                             |             |             |             |             |                         |
|---|-------------|-------------|-------------|-------------|-------------------------|
| <b>2015</b>                                       | <b>2016</b> | <b>2017</b> | <b>2018</b> | <b>2019</b> | <b>2020+ Long Range</b> |
| \$651,000   | \$106,500   | \$995,000   | \$521,000   | \$118,800   | \$312,000               |
| <b>Five Year Parks Projects Total \$2,704,300</b> |             |             |             |             |                         |



## **CHAPTER 8**

### **RESOLUTION OF ADOPTION**

**VILLAGE OF HORTONVILLE**

**RESOLUTION NO. R-15-15**

**TO ADOPT THE 2015-2019 VILLAGE OF HORTONVILLE  
COMPREHENSIVE OUTDOOR RECREATION PLAN**

**WHEREAS**, the Village of Hortonville intends to provide its residents and visitors with adequate park and recreational opportunities and protect its unique natural and cultural resources; and

**WHEREAS**, the Village of Hortonville has been working with East Central Wisconsin Regional Planning Commission in updating its Outdoor Recreation Plan; and

**WHEREAS**, the Village of Hortonville Outdoor Recreation and Leisure Advisory Committee on June 18, 2015 has recommended to the Village Board adoption of the Village of Hortonville 2015-2019 Comprehensive Outdoor and Recreation Plan; and

**WHEREAS**, the Village of Hortonville at its regular board meeting on June 18, 2015 has recommended adoption of the Village of Hortonville 2015-2019 Comprehensive Outdoor and Recreation Plan; and

**WHEREAS**, the Village of Hortonville 2015-2019 Comprehensive Outdoor and Recreation Plan encompasses a general outline and plan for the recreational development of the Village's parks system until the year 2019; and

**WHEREAS**, adoption of said plan also recommends maintaining fishing piers and docking areas, updating facilities to comply with ADA Title III rules, trail expansion efforts, a possible programs coordinator, possible splash pad / pool, ice rink, etc. in its 5 year action plan; and

**WHEREAS**, the 2015-2019 Village of Hortonville Comprehensive Outdoor Recreation Plan is an update to the 1995 version of the Outdoor Recreation Plan; and

**WHEREAS**, Village adoption of said plan is required for the Village of Hortonville to become eligible for cost sharing aid programs administered by the State of Wisconsin.

**NOW, THEREFORE, BE IT RESOLVED BY THE VILLAGE BOARD OF THE VILLAGE OF HORTONVILLE** in session this 16th day of July, 2015, that the Village of Hortonville Comprehensive Outdoor Recreation Plan (2015-2019) be adopted as the approved recreation plan for the Village of Hortonville.

By: Traci Martens  
Traci Martens, Village President

Attest: Lynne Mischker  
Lynne Mischker, WCMC, Clerk-Treasurer

This is to certify that this is a true and exact copy of Resolution R-15-15 adopted by a 6 - 0 vote of the Hortonville Village Board at its regular meeting on July 16th, 2015.



## **APPENDICES A-D**



## **APPENDIX A**

### **S.W.O.T. ANALYSIS**



**Village of Hortonville Outdoor Comprehensive Recreation Plan Update 2015-2019**

**S.W.O.T. Analysis Worksheet 4-13-15 by Committee Members**

**Strengths**

What unique recreational resources does the Village have?  
What do others see as your Village's recreational strengths?

- Black Otter Lake
- WIOUWASH State Trail
- Alonzo Park welcome to the Village
- Close to the Fox Valley
- Sports Complex at Otto Miller
- Hortonville Private Commercial Club Park
- Topography
- Events and Fresh Farmers Market
- Tot Park at Miller Park
- Golf Course
- Ball Diamonds for tournaments

**Weaknesses**

How could the Village improve its parks, rec facilities, etc.?  
Where do you have fewer facilities than others?  
What are residents likely to see as recreational weaknesses?

- No Swimming Pool
- Need more marketing and information being relayed to residents
- Trail connections lacking
- Sidewalks are limited
- No Lake Access from parks
- Black Otter is very weedy
- Kiosks Needed Downtown – Wayfinding
- More Usage of the Lake
- Winter Facilities & Activities are lacking (ie sledding hill, ice rink)
- Clean up area by creek in Miller Park to add a picnic area
- Not enough access to the WIOUWASH trail
- Recreational Programmer is lacking, no organization of events.
- Playground equipment to suit all ages (6-12 age is lacking)
- No Village Square or Center
- Wildwood Park is unusable
- Need more Basketball Courts

## Opportunities

What recreational opportunities does the Village promote?

What recreational trends could you take advantage of?

How can you turn your rec strengths into opportunities?

- Dog Park creation
- Swimming Pond/pool creation – water feature splashpad
- Sledding Hill
- Program Coordinator to organize events and youth programs
- Trail connections once bypass goes in
- Make use of what we have, lots of potential
- Promotion of events
- Sponsorships
- Education
- Close to schools
- Shoreline based trails
- Healthy benefits for trail expansion for companies – insurance rates decrease
- Camping possibility
- Only Lake in Outagamie County – promote it, clean it up.

## Threats

What threats are harming the Village's recreational areas?

What is your competition doing better, i.e. in their parks?

How are the Village's weaknesses being a threat to the parks and rec department?

- No programming
- Weedy Lake – Quality turns away visitors
- Safety concerns – sidewalks, connection to schools
- Tight budgets
- STH 15 Bypass
- Need good planning for downtown
- Missed opportunities and potential for expansion of parks system
- WDNR Regulations (Lake)
- Man Power and Staff



## **APPENDIX B**

### **SURVEY SUMMARY**

# Village of Hortonville Parks Survey 2014-2015

## Q1 What street do you live on? (Please provide N,S,E,W in your street address if known, for locational purposes only.)

Answered: 70 Skipped: 0

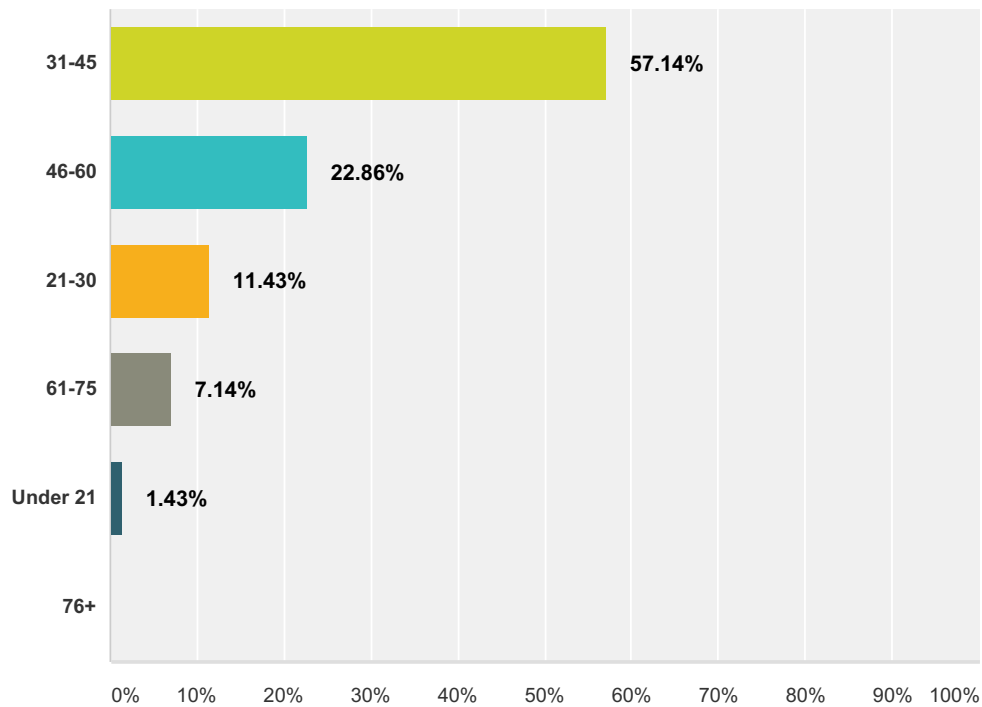
| #  | Responses   | Date                |
|----|---|---------------------|
| 1  | E Main Street                                     | 4/27/2015 11:50 AM  |
| 2  | Lakeview Avenue                                   | 4/17/2015 10:04 PM  |
| 3  | south mill  | 4/14/2015 6:34 PM   |
| 4  | Lakeview Ave                                      | 3/31/2015 11:47 PM  |
| 5  | W. MAIN ST.                                       | 3/28/2015 12:06 PM  |
| 6  | R&D   | 3/15/2015 1:48 PM   |
| 7  | n.nash  | 3/10/2015 11:00 AM  |
| 8  | W9660 Cty TT                                      | 3/9/2015 9:06 AM    |
| 9  | County road s                                     | 2/27/2015 12:21 PM  |
| 10 | Steinacker Heights                                | 2/23/2015 10:01 PM  |
| 11 | County Road S                                     | 2/23/2015 8:25 AM   |
| 12 | N3074 State hwy. 15                               | 2/12/2015 2:53 PM   |
| 13 | Mystic Drive                                      | 2/10/2015 8:55 PM   |
| 14 | W cty rd tt                                       | 2/10/2015 7:29 PM   |
| 15 | Walnut St   | 2/10/2015 6:47 PM   |
| 16 | Lakeshore Dr                                      | 2/10/2015 6:43 PM   |
| 17 | S nash street                                     | 2/10/2015 6:39 PM   |
| 18 | N. Nash   | 2/10/2015 6:23 PM   |
| 19 | Crestview dr                                      | 2/10/2015 6:22 PM   |
| 20 | E Baake Street                                    | 2/10/2015 10:07 AM  |
| 21 | W. Main Street                                    | 1/14/2015 2:36 PM   |
| 22 | ashtin way  | 12/31/2014 7:50 AM  |
| 23 | Crystal Springs Dr.                               | 12/19/2014 10:46 PM |
| 24 | Cordy Lane  | 12/19/2014 7:23 PM  |
| 25 | (West) Main Street                                | 12/10/2014 4:16 PM  |
| 26 | S Nash  | 11/17/2014 8:47 PM  |
| 27 | N2575 North Road, Hortonville (Town of Ellington) | 11/17/2014 4:57 PM  |
| 28 | N918 County Road M                                | 11/13/2014 11:11 AM |
| 29 | Honeysuckle Dr                                    | 11/7/2014 1:27 PM   |
| 30 | 805 Giese St.                                     | 10/23/2014 5:26 AM  |
| 31 | W Nye   | 10/22/2014 9:41 PM  |

## Village of Hortonville Parks Survey 2014-2015

|    |                    |                     |
|----|--------------------|---------------------|
| 32 | S. Nash            | 10/22/2014 7:31 PM  |
| 33 | Honeysuckle Dr.    | 10/22/2014 6:57 PM  |
| 34 | Parkview           | 10/22/2014 5:55 PM  |
| 35 | Mystic dr          | 10/22/2014 5:37 PM  |
| 36 | N2206 falling wing | 10/22/2014 5:18 PM  |
| 37 | Hwy 15             | 10/22/2014 2:53 PM  |
| 38 | cty rd TT          | 10/22/2014 2:20 PM  |
| 39 | Lakeview Ave       | 10/22/2014 11:34 AM |
| 40 | Town of Ellington  | 10/22/2014 11:33 AM |
| 41 | N Olk St           | 10/22/2014 11:11 AM |
| 42 | brookwood          | 10/22/2014 10:53 AM |
| 43 | Giese st           | 10/22/2014 10:43 AM |
| 44 | Brookwood Drive    | 10/22/2014 10:26 AM |
| 45 | n nye              | 10/22/2014 10:23 AM |
| 46 | Parkview Lane      | 10/20/2014 9:18 PM  |
| 47 | Spruce St          | 10/20/2014 11:28 AM |
| 48 | Lakeview Ave.      | 10/11/2014 4:47 PM  |
| 49 | Cherry             | 10/10/2014 8:23 AM  |
| 50 | S Nash st          | 10/10/2014 7:15 AM  |
| 51 | S. Nash            | 10/9/2014 10:01 PM  |
| 52 | N. Mill            | 10/9/2014 2:49 PM   |
| 53 | Brookwood          | 10/8/2014 10:07 PM  |
| 54 | Ridge Rd.          | 10/8/2014 8:43 PM   |
| 55 | Parkview Lane      | 10/8/2014 8:29 PM   |
| 56 | Pine Grove Lane    | 10/8/2014 8:16 PM   |
| 57 | Main st            | 10/8/2014 5:43 PM   |
| 58 | E cedar            | 10/8/2014 2:03 PM   |
| 59 | N2575 North rd     | 10/8/2014 12:45 PM  |
| 60 | Lakeshore          | 10/8/2014 12:12 PM  |
| 61 | S mill street      | 10/8/2014 11:25 AM  |
| 62 | lakeshore dr       | 10/8/2014 11:18 AM  |
| 63 | Michael Ritger St  | 10/8/2014 10:38 AM  |
| 64 | R and d rd         | 10/8/2014 10:35 AM  |
| 65 | S. Mill St         | 10/8/2014 10:17 AM  |
| 66 | Giese              | 10/8/2014 9:55 AM   |
| 67 | S Lake St          | 10/8/2014 9:43 AM   |
| 68 | Birch St           | 10/8/2014 9:25 AM   |
| 69 | emily way          | 10/6/2014 3:40 PM   |

### Q2 How old is the person filling out this survey?

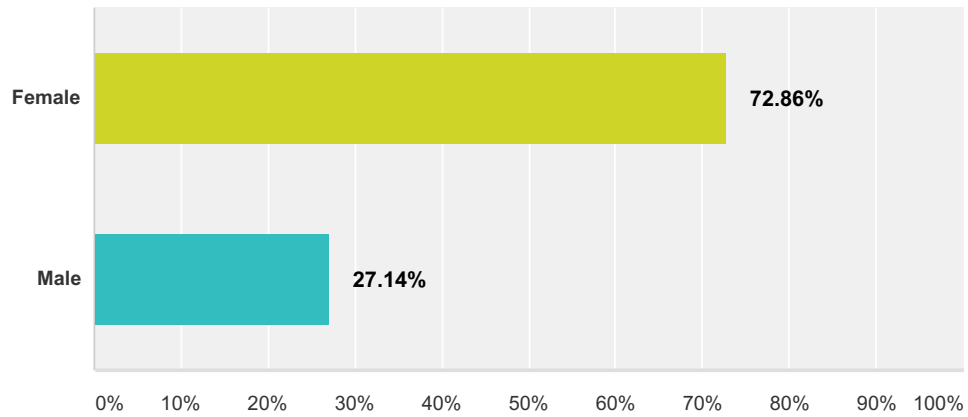
Answered: 70 Skipped: 0



| Answer Choices | Responses |    |
|----------------|-----------|----|
| 31-45          | 57.14%    | 40 |
| 46-60          | 22.86%    | 16 |
| 21-30          | 11.43%    | 8  |
| 61-75          | 7.14%     | 5  |
| Under 21       | 1.43%     | 1  |
| 76+            | 0.00%     | 0  |
| Total          |           | 70 |

### Q3 Are you male or female?

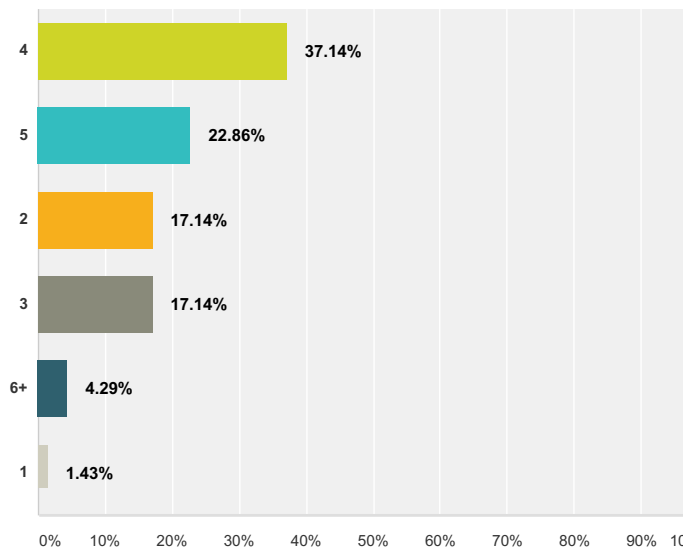
Answered: 70 Skipped: 0



| Answer Choices | Responses |    |
|----------------|-----------|----|
| Female         | 72.86%    | 51 |
| Male           | 27.14%    | 19 |
| Total          |           | 70 |

### Q4 How many people reside in your household?

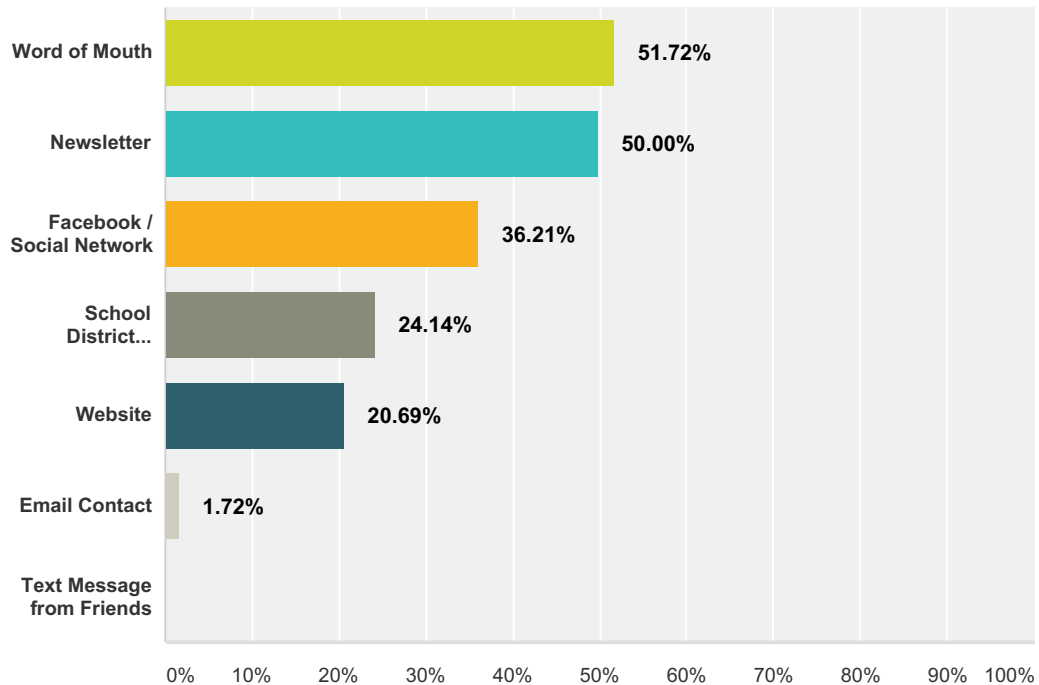
Answered: 70 Skipped: 0



| Answer Choices | Responses |
|----------------|-----------|
| 4              | 37.14%    |
| 5              | 22.86%    |
| 2              | 17.14%    |
| 3              | 17.14%    |
| 6+             | 4.29%     |
| 1              | 1.43%     |
| Total          |           |

**Q5 How do you get information about Hortonville parks and recreation programs or activities? (Please check all that apply.)**

Answered: 58 Skipped: 12



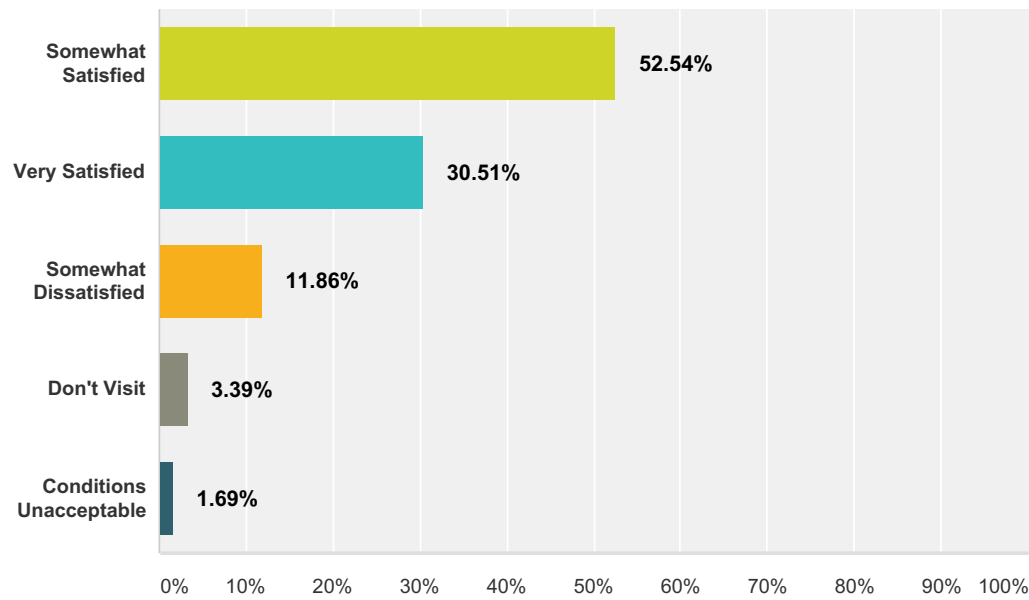
| Answer Choices              | Responses |
|-----------------------------|-----------|
| Word of Mouth               | 51.72% 30 |
| Newsletter                  | 50.00% 29 |
| Facebook / Social Network   | 36.21% 21 |
| School District Information | 24.14% 14 |
| Website                     | 20.69% 12 |
| Email Contact               | 1.72% 1   |
| Text Message from Friends   | 0.00% 0   |
| Total Respondents: 58       |           |

| # | Other / Comments | Date                |
|---|------------------|---------------------|
| 1 | Roadside Signage | 4/17/2015 10:14 PM  |
| 2 | library          | 10/22/2014 10:58 AM |



Q6 What is your overall satisfaction with the condition of the parks you visit?

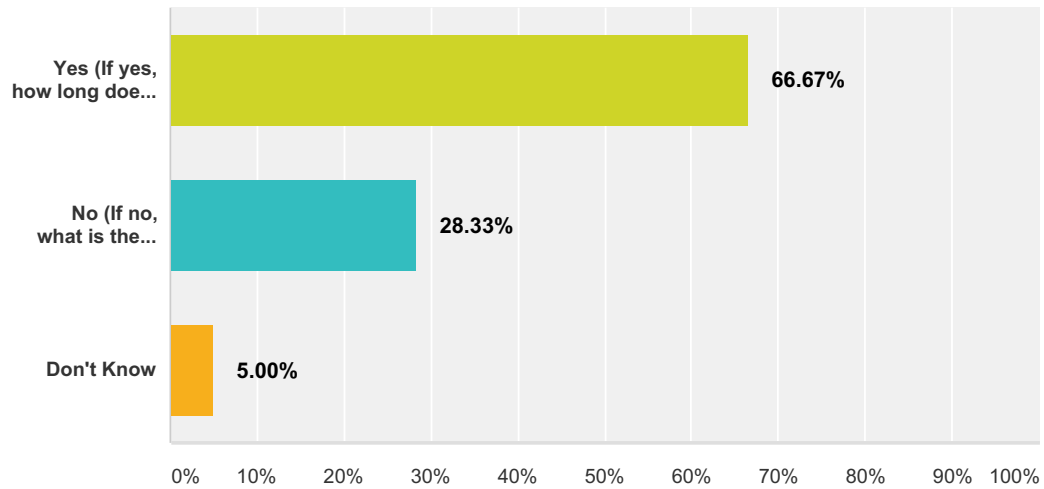
Answered: 59 Skipped: 11



| Answer Choices          | Responses |    |
|-------------------------|-----------|----|
| Somewhat Satisfied      | 52.54%    | 31 |
| Very Satisfied          | 30.51%    | 18 |
| Somewhat Dissatisfied   | 11.86%    | 7  |
| Don't Visit             | 3.39%     | 2  |
| Conditions Unacceptable | 1.69%     | 1  |
| Total                   |           | 59 |

## Q7 Currently, are you able to safely walk and/or bike to a park or recreational facility?

Answered: 60 Skipped: 10



| Answer Choices   | Responses |
|--|-----------|
| Yes (If yes, how long does it take?, Please specify below) | 66.67% 40 |
| No (If no, what is the barrier?, Please specify below)     | 28.33% 17 |
| Don't Know   | 5.00% 3   |
| <b>Total</b>   | <b>60</b> |

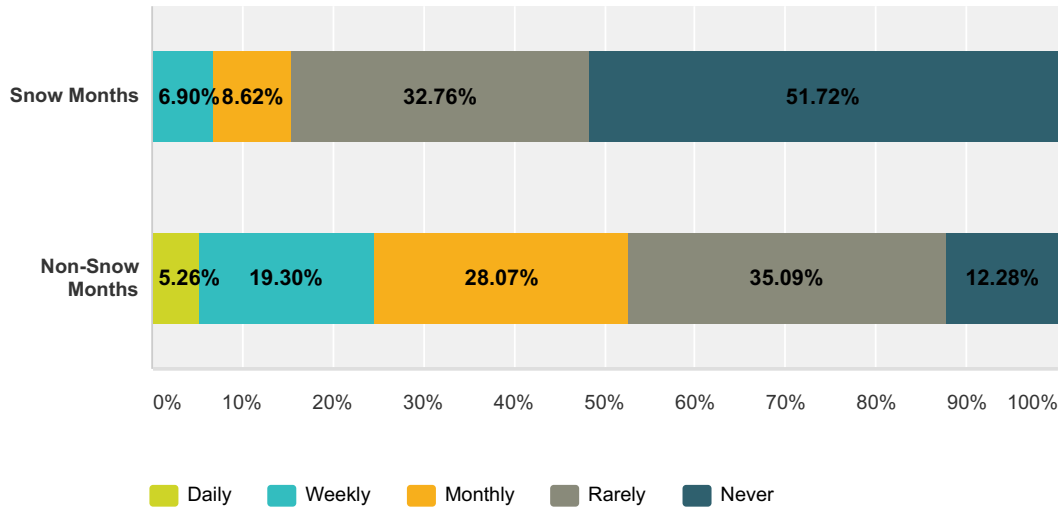
| #  | Time / Barrier?   | Date               |
|----|---|--------------------|
| 1  | about 10 minutes  | 4/27/2015 11:53 AM |
| 2  | Bike- Approximately 5 minutes/Walk- Approximately 10 minutes  | 4/17/2015 10:14 PM |
| 3  | 5 min.  | 4/14/2015 6:39 PM  |
| 4  | 5-7 minutes   | 3/31/2015 11:53 PM |
| 5  | 15 minutes. Would like to see a dog park.   | 3/28/2015 12:09 PM |
| 6  | There are no bike trails and cars are traveling at 55 mph   | 3/9/2015 9:17 AM   |
| 7  | We live on the highway!   | 2/12/2015 2:57 PM  |
| 8  | 20 min each way   | 2/10/2015 9:14 PM  |
| 9  | Lack of sidewalks   | 2/10/2015 7:35 PM  |
| 10 | 15 min  | 2/10/2015 7:01 PM  |
| 11 | 10 mins   | 2/10/2015 6:44 PM  |
| 12 | no sidewalk, crossing busy intersections (M and 15), the corner of TT and M is very dangerous for myself and my family                                  | 2/10/2015 10:11 AM |
| 13 | no sidewalk to cross the railroad tracks at M/Black Otter Supper Club, sidewalk does not extend all the way south on M to either Jacquot or Honeysuckle | 12/31/2014 7:54 AM |

## Village of Hortonville Parks Survey 2014-2015

|    |  |                     |
|----|--|---------------------|
| 14 | Less than 10 minutes   | 12/19/2014 10:51 PM |
| 15 | No sidewalks out to my house   | 12/10/2014 4:21 PM  |
| 16 | 20 minutes   | 11/17/2014 8:52 PM  |
| 17 | 30 seconds   | 11/13/2014 11:17 AM |
| 18 | Distance and traffic.  | 11/7/2014 1:32 PM   |
| 19 | 5 min  | 10/22/2014 9:49 PM  |
| 20 | Having to ride/ walk in traffic  | 10/22/2014 7:38 PM  |
| 21 | 10 min but busy hey 15   | 10/22/2014 5:22 PM  |
| 22 | Live on busy road.no sidewalk or trail to get to town  | 10/22/2014 2:58 PM  |
| 23 | It takes me 20 minutes but we live a couple of miles from town and I will not let my children ride down Grandview Rd by themselves. Grandview Rd (in the Village) is too narrow and there isn't a bike lane. | 10/22/2014 11:37 AM |
| 24 | parks I want to go to are too far away with small child to walk (alonzo or commercial club)  | 10/22/2014 10:58 AM |
| 25 | 10 mins walking  | 10/22/2014 10:46 AM |
| 26 | 20 minutes   | 10/22/2014 10:30 AM |
| 27 | 5 min  | 10/22/2014 10:27 AM |
| 28 | 5-10 minutes   | 10/20/2014 11:33 AM |
| 29 | 10 min.  | 10/11/2014 4:51 PM  |
| 30 | Just a few minutes   | 10/10/2014 8:27 AM  |
| 31 | 15-20 mins   | 10/10/2014 7:23 AM  |
| 32 | 15 minutes   | 10/9/2014 10:06 PM  |
| 33 | 15-20 minutes  | 10/8/2014 8:47 PM   |
| 34 | No sidewalk to park - would have to walk on highway. They used to mow a path between Lake Street and the park, but did not do this summer.   | 10/8/2014 8:22 PM   |
| 35 | 5 min  | 10/8/2014 2:12 PM   |
| 36 | 20 minutes   | 10/8/2014 12:48 PM  |
| 37 | Roads and crosswalks too busy  | 10/8/2014 12:15 PM  |
| 38 | 5 min  | 10/8/2014 11:31 AM  |
| 39 | no side walk from spruce st to the tracks  | 10/8/2014 10:43 AM  |
| 40 | Off of m   | 10/8/2014 10:39 AM  |
| 41 | 5 minutes  | 10/8/2014 9:46 AM   |
| 42 | There are no sidewalks on Main St to get from my house to Miller Park. The other parks are too far for walking distance.   | 10/8/2014 9:27 AM   |
| 43 | 5-10 mins  | 10/6/2014 3:44 PM   |
| 44 | We live out of town but visit Hortonville very frequently use the parks there. We find access is fine once we are in the Village.  | 9/23/2014 10:22 AM  |

**Q8 In the past 12 months, looking at season, how often has you or one of your family members used the trails in Hortonville for walking or biking?**

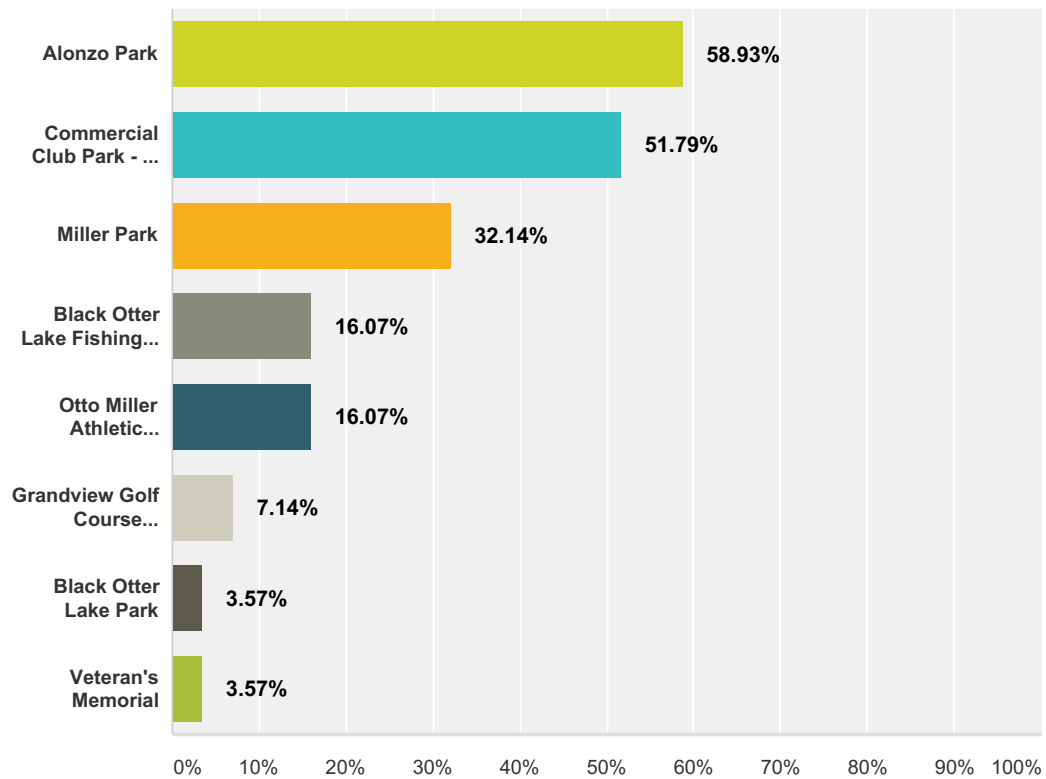
Answered: 59 Skipped: 11



|                 | Daily      | Weekly       | Monthly      | Rarely       | Never        | Total |
|-----------------|------------|--------------|--------------|--------------|--------------|-------|
| Snow Months     | 0.00%<br>0 | 6.90%<br>4   | 8.62%<br>5   | 32.76%<br>19 | 51.72%<br>30 | 58    |
| Non-Snow Months | 5.26%<br>3 | 19.30%<br>11 | 28.07%<br>16 | 35.09%<br>20 | 12.28%<br>7  | 57    |

### Q9 What is/are your favorite park or parks in Hortonville? Please mark all that apply.

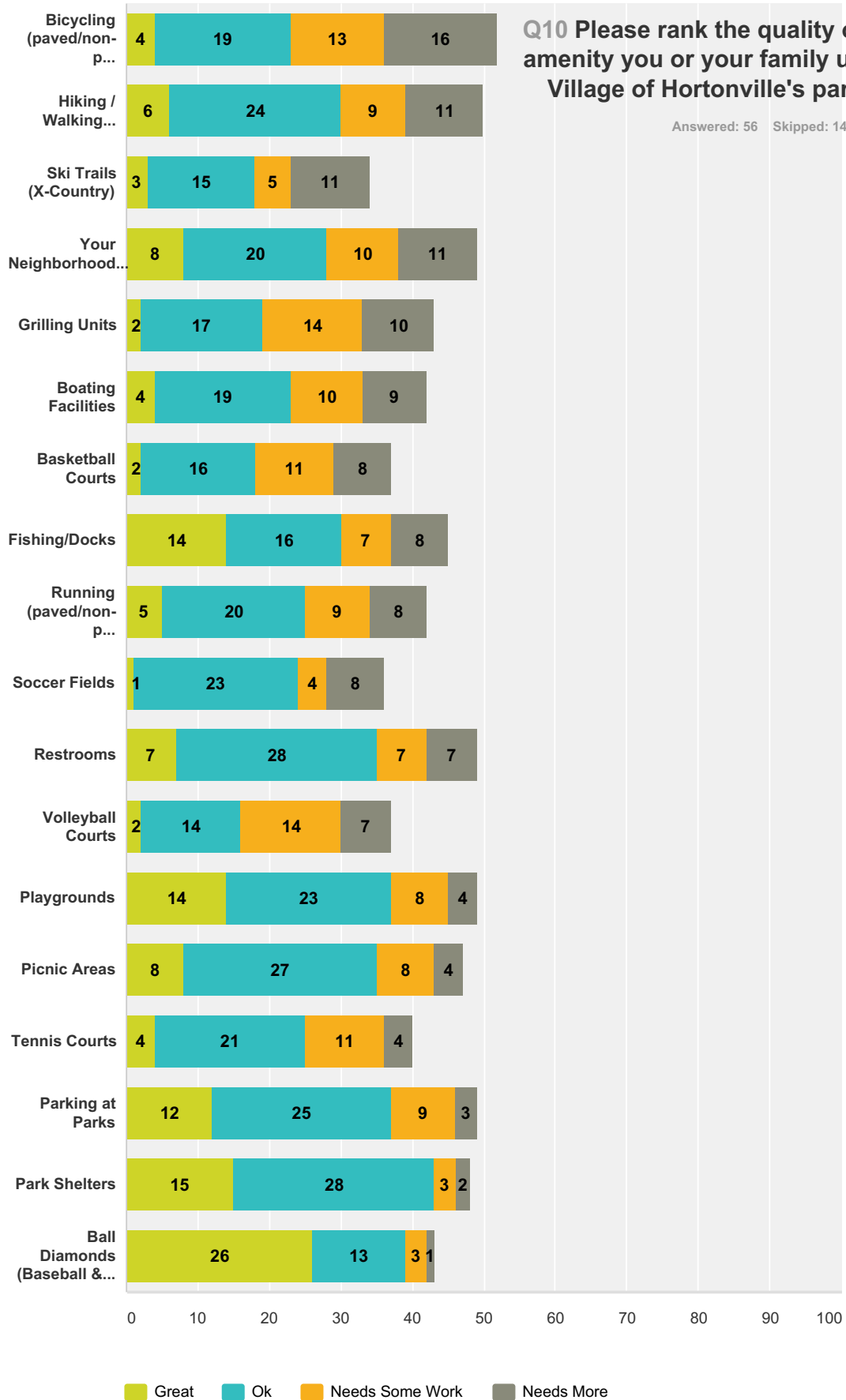
Answered: 56 Skipped: 14



| Answer Choices                       | Responses |
|--------------------------------------|-----------|
| Alonzo Park                          | 58.93% 33 |
| Commercial Club Park - HCC (private) | 51.79% 29 |
| Miller Park                          | 32.14% 18 |
| Black Otter Lake Fishing Pier        | 16.07% 9  |
| Otto Miller Athletic Complex         | 16.07% 9  |
| Grandview Golf Course (private)      | 7.14% 4   |
| Black Otter Lake Park                | 3.57% 2   |
| Veteran's Memorial                   | 3.57% 2   |
| Total Respondents: 56                |           |

| # | Other (please specify) | Date                |
|---|------------------------|---------------------|
| 1 | Armtage Park           | 11/13/2014 11:17 AM |
| 2 | Wiowash Trail          | 10/22/2014 11:37 AM |
| 3 | None                   | 10/9/2014 2:50 PM   |

## Village of Hortonville Parks Survey 2014-2015



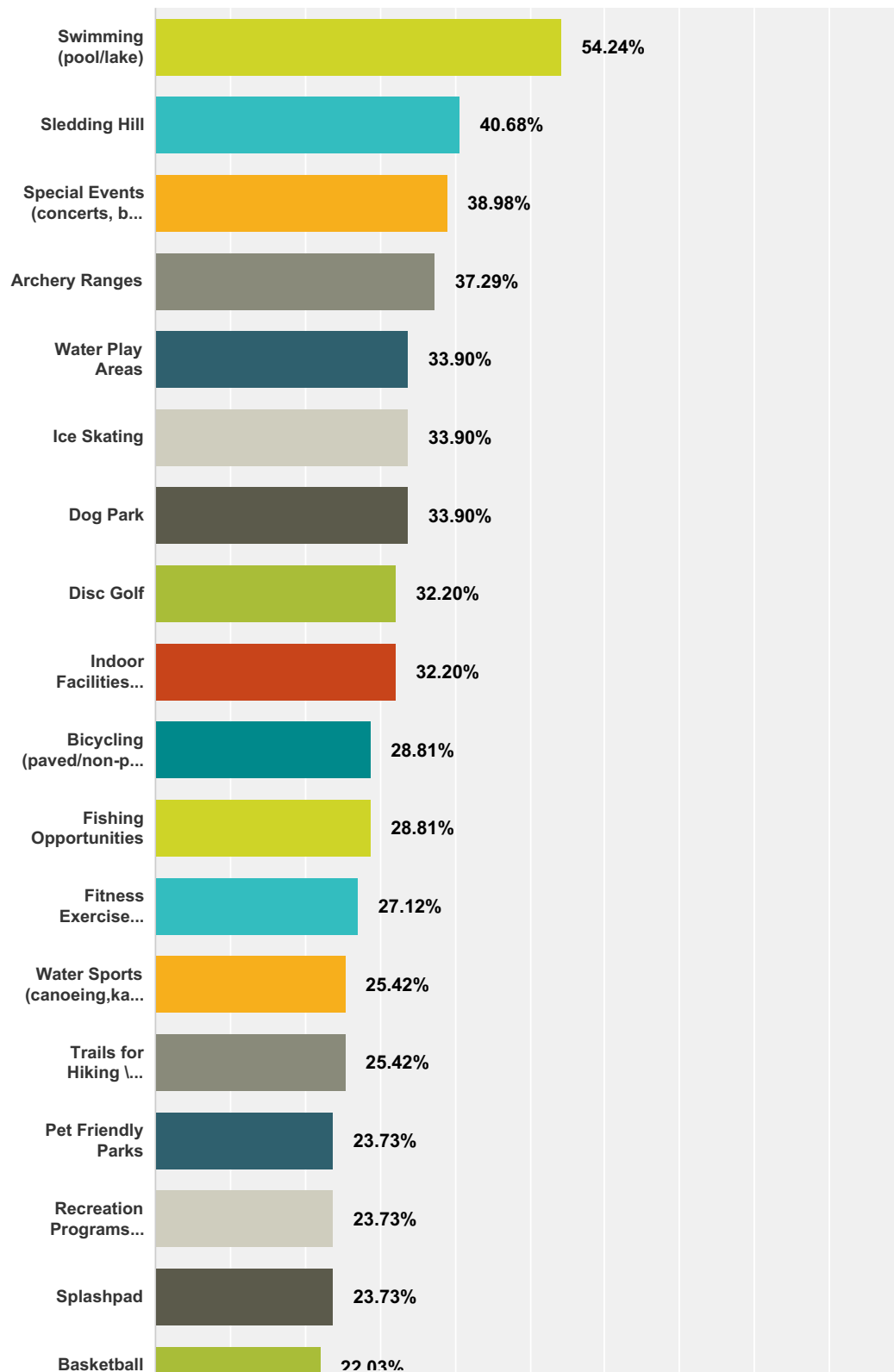
## Village of Hortonville Parks Survey 2014-2015

|   | Great        | Ok           | Needs Some Work | Needs More   | Total | Weighted Average |
|---|--------------|--------------|-----------------|--------------|-------|------------------|
| Bicycling (paved/non-paved trails/on-street)  | 7.69%<br>4   | 36.54%<br>19 | 25.00%<br>13    | 30.77%<br>16 | 52    | 2.79             |
| Hiking / Walking (paved/non-paved trails)     | 12.00%<br>6  | 48.00%<br>24 | 18.00%<br>9     | 22.00%<br>11 | 50    | 2.50             |
| Ski Trails (X-Country)                        | 8.82%<br>3   | 44.12%<br>15 | 14.71%<br>5     | 32.35%<br>11 | 34    | 2.71             |
| Your Neighborhood's access to park facilities | 16.33%<br>8  | 40.82%<br>20 | 20.41%<br>10    | 22.45%<br>11 | 49    | 2.49             |
| Grilling Units                                | 4.65%<br>2   | 39.53%<br>17 | 32.56%<br>14    | 23.26%<br>10 | 43    | 2.74             |
| Boating Facilities                            | 9.52%<br>4   | 45.24%<br>19 | 23.81%<br>10    | 21.43%<br>9  | 42    | 2.57             |
| Basketball Courts                             | 5.41%<br>2   | 43.24%<br>16 | 29.73%<br>11    | 21.62%<br>8  | 37    | 2.68             |
| Fishing/Docks                                 | 31.11%<br>14 | 35.56%<br>16 | 15.56%<br>7     | 17.78%<br>8  | 45    | 2.20             |
| Running (paved/non-paved trails)              | 11.90%<br>5  | 47.62%<br>20 | 21.43%<br>9     | 19.05%<br>8  | 42    | 2.48             |
| Soccer Fields                                 | 2.78%<br>1   | 63.89%<br>23 | 11.11%<br>4     | 22.22%<br>8  | 36    | 2.53             |
| Restrooms                                     | 14.29%<br>7  | 57.14%<br>28 | 14.29%<br>7     | 14.29%<br>7  | 49    | 2.29             |
| Volleyball Courts                             | 5.41%<br>2   | 37.84%<br>14 | 37.84%<br>14    | 18.92%<br>7  | 37    | 2.70             |
| Playgrounds                                   | 28.57%<br>14 | 46.94%<br>23 | 16.33%<br>8     | 8.16%<br>4   | 49    | 2.04             |
| Picnic Areas                                  | 17.02%<br>8  | 57.45%<br>27 | 17.02%<br>8     | 8.51%<br>4   | 47    | 2.17             |
| Tennis Courts                                 | 10.00%<br>4  | 52.50%<br>21 | 27.50%<br>11    | 10.00%<br>4  | 40    | 2.38             |
| Parking at Parks                              | 24.49%<br>12 | 51.02%<br>25 | 18.37%<br>9     | 6.12%<br>3   | 49    | 2.06             |
| Park Shelters                                 | 31.25%<br>15 | 58.33%<br>28 | 6.25%<br>3      | 4.17%<br>2   | 48    | 1.83             |
| Ball Diamonds (Baseball & Softball)           | 60.47%<br>26 | 30.23%<br>13 | 6.98%<br>3      | 2.33%<br>1   | 43    | 1.51             |

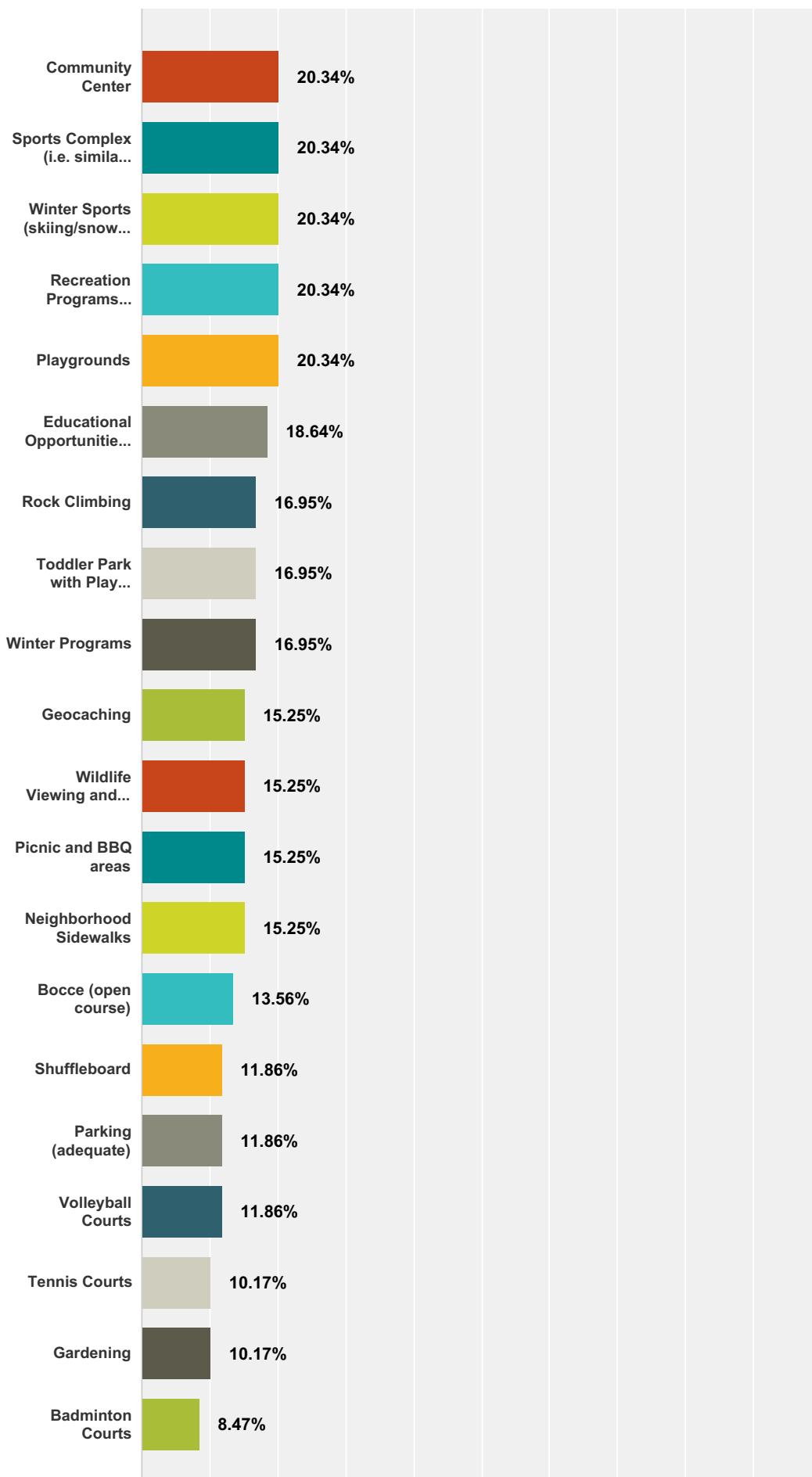


**Q11 Please choose which activities you would like to see ADDED or EXPANDED to Hortonville's recreational system. (Mark all that apply.)**

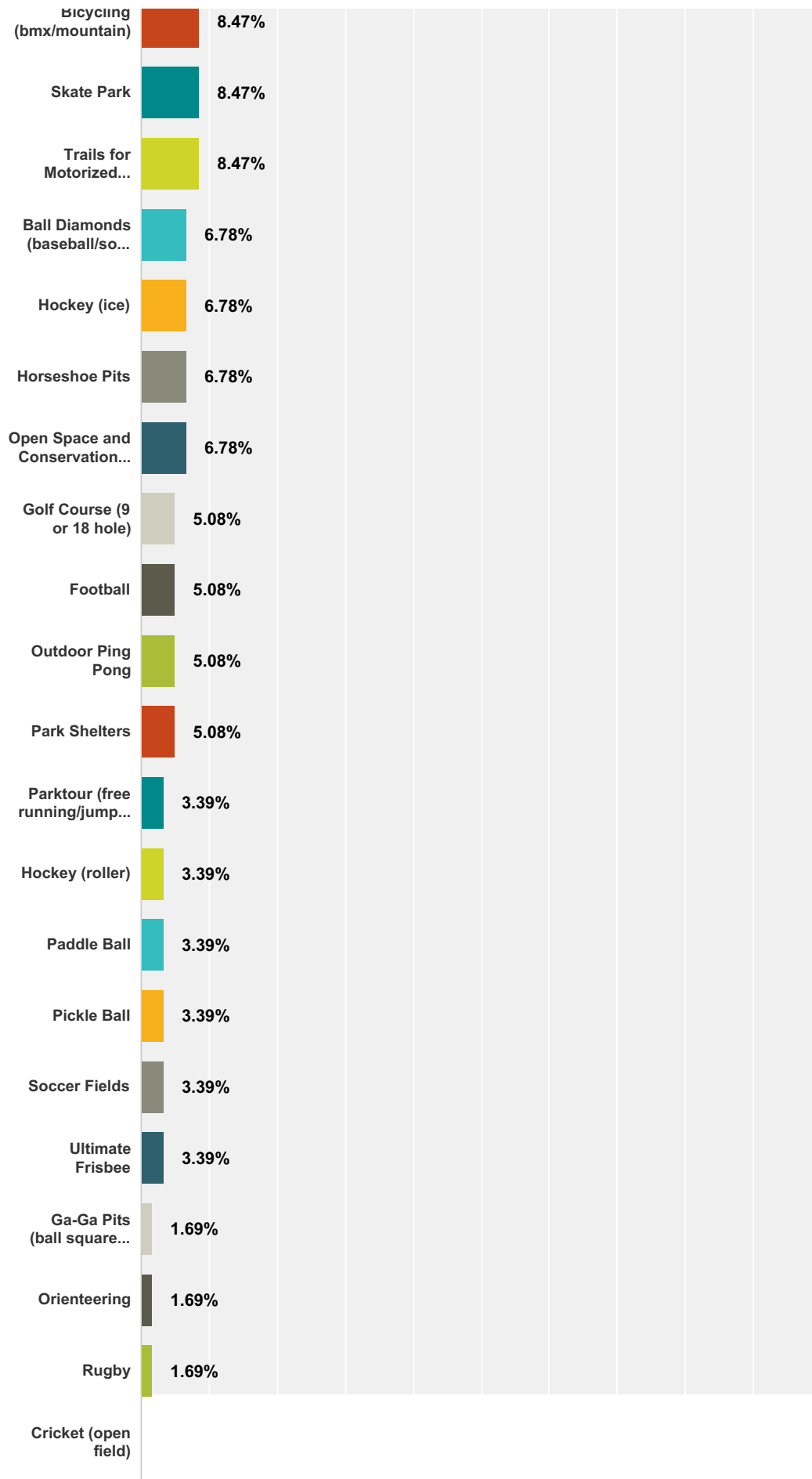
Answered: 59 Skipped: 11



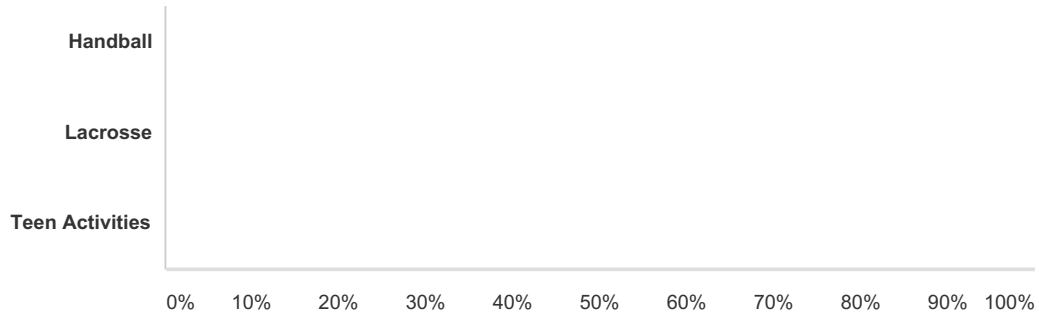
## Village of Hortonville Parks Survey 2014-2015



## Village of Hortonville Parks Survey 2014-2015



## Village of Hortonville Parks Survey 2014-2015



| Answer Choices  | Responses |    |
|---|-----------|----|
| Swimming (pool/lake)  | 54.24%    | 32 |
| Sledding Hill   | 40.68%    | 24 |
| Special Events (concerts, brat frys, etc.)                            | 38.98%    | 23 |
| Archery Ranges  | 37.29%    | 22 |
| Water Play Areas  | 33.90%    | 20 |
| Ice Skating   | 33.90%    | 20 |
| Dog Park  | 33.90%    | 20 |
| Disc Golf   | 32.20%    | 19 |
| Indoor Facilities (gym,fitness center)                                | 32.20%    | 19 |
| Bicycling (paved/non-paved trails/on-street)                          | 28.81%    | 17 |
| Fishing Opportunities   | 28.81%    | 17 |
| Fitness Exercise (course/outside)                                     | 27.12%    | 16 |
| Water Sports (canoeing,kayaking,paddle boarding)                      | 25.42%    | 15 |
| Trails for Hiking \ Walking   | 25.42%    | 15 |
| Pet Friendly Parks  | 23.73%    | 14 |
| Recreation Programs (Youth)   | 23.73%    | 14 |
| Splashpad   | 23.73%    | 14 |
| Basketball Courts   | 22.03%    | 13 |
| Community Center  | 20.34%    | 12 |
| Sports Complex (i.e. similar to USA Youth Sports Complex in Appleton) | 20.34%    | 12 |
| Winter Sports (skiing/snowshoeing)                                    | 20.34%    | 12 |
| Recreation Programs (Adult)   | 20.34%    | 12 |
| Playgrounds   | 20.34%    | 12 |
| Educational Opportunities (nature courses, etc.)                      | 18.64%    | 11 |

## Village of Hortonville Parks Survey 2014-2015

|  |        |    |
|--|--------|----|
| Rock Climbing                            | 16.95% | 10 |
| Toddler Park with Play Equipment         | 16.95% | 10 |
| Winter Programs                          | 16.95% | 10 |
| Geocaching                               | 15.25% | 9  |
| Wildlife Viewing and bird watching       | 15.25% | 9  |
| Picnic and BBQ areas                     | 15.25% | 9  |
| Neighborhood Sidewalks                   | 15.25% | 9  |
| Bocce (open course)                      | 13.56% | 8  |
| Shuffleboard                             | 11.86% | 7  |
| Parking (adequate)                       | 11.86% | 7  |
| Volleyball Courts                        | 11.86% | 7  |
| Tennis Courts                            | 10.17% | 6  |
| Gardening                                | 10.17% | 6  |
| Badminton Courts                         | 8.47%  | 5  |
| Bicycling (bmx/mountain)                 | 8.47%  | 5  |
| Skate Park                               | 8.47%  | 5  |
| Trails for Motorized Vehicles            | 8.47%  | 5  |
| Ball Diamonds (baseball/softball)        | 6.78%  | 4  |
| Hockey (ice)                             | 6.78%  | 4  |
| Horseshoe Pits                           | 6.78%  | 4  |
| Open Space and Conservation Areas        | 6.78%  | 4  |
| Golf Course (9 or 18 hole)               | 5.08%  | 3  |
| Football                                 | 5.08%  | 3  |
| Outdoor Ping Pong                        | 5.08%  | 3  |
| Park Shelters                            | 5.08%  | 3  |
| Parktour (free running/jumping/climbing) | 3.39%  | 2  |
| Hockey (roller)                          | 3.39%  | 2  |
| Paddle Ball                              | 3.39%  | 2  |
| Pickle Ball                              | 3.39%  | 2  |
| Soccer Fields                            | 3.39%  | 2  |
| Ultimate Frisbee                         | 3.39%  | 2  |
| Ga-Ga Pits (ball square game in a pit)   | 1.69%  | 1  |

## Village of Hortonville Parks Survey 2014-2015

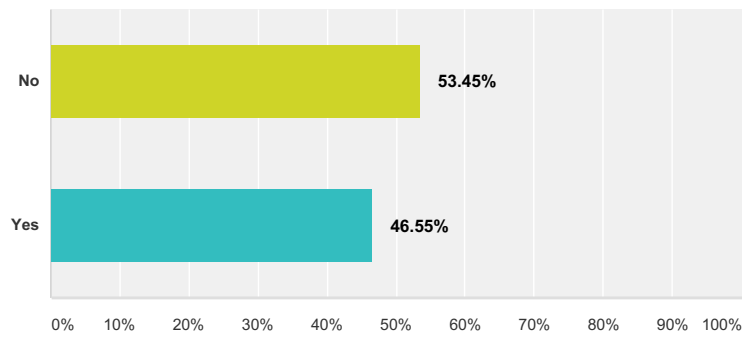
|                              |       |   |
|------------------------------|-------|---|
| Orienteering                 | 1.69% | 1 |
| Rugby                        | 1.69% | 1 |
| Cricket (open field)         | 0.00% | 0 |
| Handball                     | 0.00% | 0 |
| Lacrosse                     | 0.00% | 0 |
| Teen Activities              | 0.00% | 0 |
| <b>Total Respondents: 59</b> |       |   |

| # | Other (please specify) / Comments   | Date                |
|---|-------------------------------------|---------------------|
| 1 | Outdoor Music Park                  | 2/10/2015 9:14 PM   |
| 2 | aerobic classes (zumba, step, yoga) | 10/22/2014 10:58 AM |
| 3 | Number 1 would be a pool            | 10/8/2014 8:47 PM   |
| 4 | Pool                                | 10/8/2014 9:27 AM   |

# Village of Hortonville Parks Survey 2014-2015

## Q12 Do you feel more parks are needed in Hortonville?

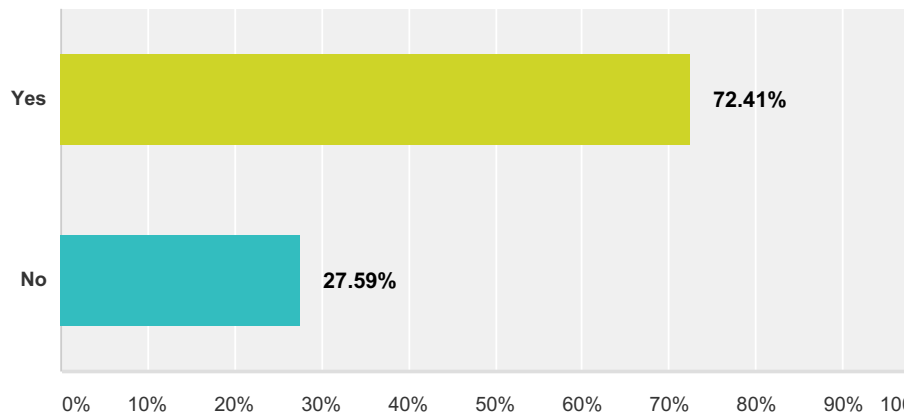
Answered: 58 Skipped: 12



| Answer Choices | Responses |
|----------------|-----------|
| No             | 53.45% 31 |
| Yes            | 46.55% 27 |
| Total          | 58        |

## Q13 Do you feel more trails are needed?

Answered: 58 Skipped: 12



| Answer Choices | Responses |
|----------------|-----------|
| Yes            | 72.41% 42 |
| No             | 27.59% 16 |
| Total          | 58        |



## Village of Hortonville Parks Survey 2014-2015

### Q14 Please provide any other comments to help us improve the parks and recreational services for the Village of Hortonville.

Answered: 22   Skipped: 48

| #  | Responses   | Date                |
|----|---|---------------------|
| 1  | Please consider coordinating with Outagamie County to place large boulders around the center court (on the grass area) within the parking lot at the beginning/end of the Wiouwash State Trail. Too often the grass area is damaged by individuals trying to have fun with their cars and trucks and it is turned into a big muddy mess. Thank you!   | 4/17/2015 10:26 PM  |
| 2  | Would be very nice to see a fenced in dog park with a small area to grill and picnic area with one bathroom would be very nice. It would be nice to see a local dog park added to an area close to another park for kids to play. Not many close dog parks to hortonville.  | 3/28/2015 12:11 PM  |
| 3  | This is a great start asking people what they are interested in.  | 3/9/2015 9:19 AM    |
| 4  | Thank you for pursuing this very important project! People are becoming more and more health conscious. Providing our community with more recreational opportunities will make our community a better place to live and will attract other people to want to live here.   | 2/23/2015 10:09 AM  |
| 5  | We currently do not use any of the parks regularly. We travel to Greenville and Appleton to go to sledding hills and ice skating rinks and frisbee golf. There is theoretically a trail out of our street to connect to the next subdivision over, but we can not find it. Looking specifically for more things to do with kids 8-14 once they do not play on playgrounds as much.                            | 2/10/2015 9:20 PM   |
| 6  | My kids enjoyed when the library had the book to read while walking the trail around Alonzo park. They always ask if they are going to do that again. It was a great idea!  | 2/10/2015 7:37 PM   |
| 7  | I thinking having one less park and having a swimming area for the summer would be a great thing for the whole community.   | 2/10/2015 6:45 PM   |
| 8  | The Village really needs an ice skating rink. I would love to see an outdoor hockey rink but at least start with a basic sheet of ice for ice skating. Even in winters where there is not a lot of snow people could go out and ice skate. Alonzo park would be a great location for ice skating. Additionally, it would be close to the school so gym classes could go out and take lessons in ice skating.  | 1/14/2015 2:39 PM   |
| 9  | If we could give Alonzo a good overhaul, move the parking lot over away from the stoplight, add additional activities- it would make a huge difference coming into Hortonville! It's not that we need more parks, it's that we need to give our existing parks a facelift and spruce them up for our community!   | 12/10/2014 4:23 PM  |
| 10 | I would like to see LESS dogs running around in childrens parks. It is discusting to see dog dropping all over and children stepping in it. Need a dog park of keep dogs out of childrens areas please. It is a health issue also. Think of the children and not the animals. Yes, I do love dogs but they do not belong in the parks. Everyone lets them run free and that is not a dumping ground for them. | 11/13/2014 11:20 AM |
| 11 | Great job by Hortonville staff in the parks!  | 11/7/2014 1:32 PM   |
| 12 | Archery is a very popular sport and would do well. The weeds on the lake have to be controlled . People comment about how bad it looks and it is tough to fish in the summer. I would donate some to a archery range. People would pay to do the course which would of set the cost. All courses have donation boxes.   | 10/22/2014 9:56 PM  |
| 13 | No dog park   | 10/22/2014 5:22 PM  |
| 14 | There are areas within the Village that do not have sidewalks. It is difficult to walk around town without them. It would also be great if there were some bike paths to the Wiowash Trail.   | 10/22/2014 11:38 AM |
| 15 | A swimming pool and swimming lessons would be awesome; I'd rather have my kids learn to swim here vs driving to New London :)   | 10/22/2014 10:31 AM |
| 16 | Improve the quality of fishing in the lake.   | 10/20/2014 11:34 AM |

## Village of Hortonville Parks Survey 2014-2015

|    |  |                    |
|----|--|--------------------|
| 17 | Keep it family oriented. Hortonville is a young family community right now And things for toddlers to teens in our community would be great so we don't have to leave for new London and appleton!   | 10/10/2014 7:24 AM |
| 18 | You suck   | 10/9/2014 2:50 PM  |
| 19 | Upkeep of parks is horrible . You need to fire whoever miss the grass especially in the cemetery. Broken equipment seldom gets repaired. Bathrooms in public parks get locked . For the amount we pay in taxes compared to other towns we should have much nicer facilitates. It's like all the money is going to the new "community" center. I know who decides what happens in hortonville and it not the residents. | 10/8/2014 12:21 PM |
| 20 | Do better at up keep an area for kids to swim would b great our lake is an embarrassment   | 10/8/2014 10:40 AM |
| 21 | We visit the parks often in the summer. I would love to have a pool or spalshpad for hot summer days. I also think better access where there are no sidewalks or new park areas would be great. Also, is there a Facebook page?  | 10/8/2014 9:28 AM  |
| 22 | Keep up the good work and complete the planning process for the Outdoor Rec. plan. Hortonville is already a wonderful community to visit and spend time quality time. Our family lives in New London, we do not have the time or resources to take long destination vacations. We do however love "day-cations" to the Village of Hortonville.   | 9/23/2014 10:27 AM |





## **APPENDIX C**

### TRAIL PLANNING & CONNECTIONS REPORT

# VILLAGE OF HORTONVILLE TRAIL PLANNING & CONNECTIONS REPORT

Prepared: September 6, 2013



Prepared By:  
East Central Wisconsin  
Regional Planning Commission

# **Village of Hortonville**

Outagamie County, Wisconsin

## **2015 Village Board**

|                 |           |
|-----------------|-----------|
| Traci Martens   | President |
| Tori Mann       | Trustee   |
| Al Habeck       | Trustee   |
| Peter Olk       | Trustee   |
| Kelly Schleif   | Trustee   |
| Jeff Schuh      | Trustee   |
| Dawn Vollbrecht | Trustee   |

## **Village Staff**

|              |                          |
|--------------|--------------------------|
| Vacant       | Administrator            |
| Carl McCrary | Director of Public Works |

# ABSTRACT

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**Title:** Village of Hortonville Trail Planning & Connections Report

**Author(s):** David Kress, Planner

**Subject:** Multi-modal connections within the community.

**Publication Date:** September 6, 2013

**File Name:** V.Hortonville\_TrailPlanning+ConnectionsReport.pub (or .pdf)

**Source of Copies:** East Central Wisconsin Regional Planning Commission  
400 Ahnaip Street, Suite 100  
Menasha, WI 54952  
Phone: 920.751.4770  
Fax: 920.751.4771  
Website: [www.ecwrpc.org](http://www.ecwrpc.org)

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# INTRODUCTION & PROJECT DESCRIPTION

## Purpose

As the starting point of the southern segment of the Wiouwash State Trail, the Village of Hortonville has the opportunity to capitalize on this community asset's potential. Therefore, the Village wishes to connect the trailhead, located on its south side, to several existing community destinations – namely Alonzo & Black Otter Parks and downtown. Doing so will produce a number of livability and economic benefits for Hortonville residents, trail users, and local businesses. These include: greater access to and expansion of the area's recreational network, safer linkages throughout the community, and increased use of existing facilities. In addition, the local economy stands to benefit from these connections, as they will help draw more people (and spending power) to downtown restaurants, stores, and other places of business.

Trails and other multi-modal facilities are becoming increasingly popular nationwide, as they add value to a community for the following basic reasons (modified from *Iowa Trails 2000* – <http://www.iowadot.gov/iowabikes/trails/iowatrails2000.html>):

- Health, Fitness, & Quality of Life
  - Active recreation, in any form, is a proven health benefit. Trails and other multi-modal facilities allow people the opportunity to recreate in a variety of ways, depending on their abilities and preferences.
  - Neighborhoods with trail connections are desirable places to live, and communities with high-quality trail networks are attracting new businesses and residents. The quality of life of people with access to trails is based on health, fitness, and their ability to step outdoors and appreciate scenic beauty.
- Transportation
  - The increasing number of automobiles on our roadways causes congestion. An increase in the number of trips made by pedestrians and bicycles helps to improve

air quality and reduce congestion.

- If trails connect residential neighborhoods, recreational areas, commercial and employment centers, and schools, they can offer transportation choices to users.
- Open Space & Natural Area Preservation
  - By implementing a continuous recreational corridor, a community can connect its existing green spaces and enhance the linear green space between them.
  - Establishing trails as both recreational and ecological corridors helps increase the amount and quality of open space and natural areas in a community.
- Economic Development
  - Trail users spend money. Such expenditures may range from snacks or drinks to bicycle repair to overnight stays. Communities that serve as trailheads are poised to take advantage of this economic inflow.
  - Trails and other multi-modal facilities can give people a reason to visit a community, and may spur other benefits -- such as downtown revitalization, an increase in property values, and attraction of additional businesses -- whether they serve the trail or not.
- User Safety
  - Trails and other multi-modal facilities offer dedicated travel routes for walkers, bicyclists, equestrians, and snowmobilers, thereby reducing conflicts with vehicular traffic.

To achieve the goals and benefits listed above, this report, prepared by the East Central Wisconsin Regional Planning Commission as a part of their technical assistance program, will provide the Village with general concepts and considerations for making these important connections. Therefore, this document should be seen as a “road map” to help refine trail planning ideas, identify and solicit funding opportunities, and increase public awareness of the projects’ potential.

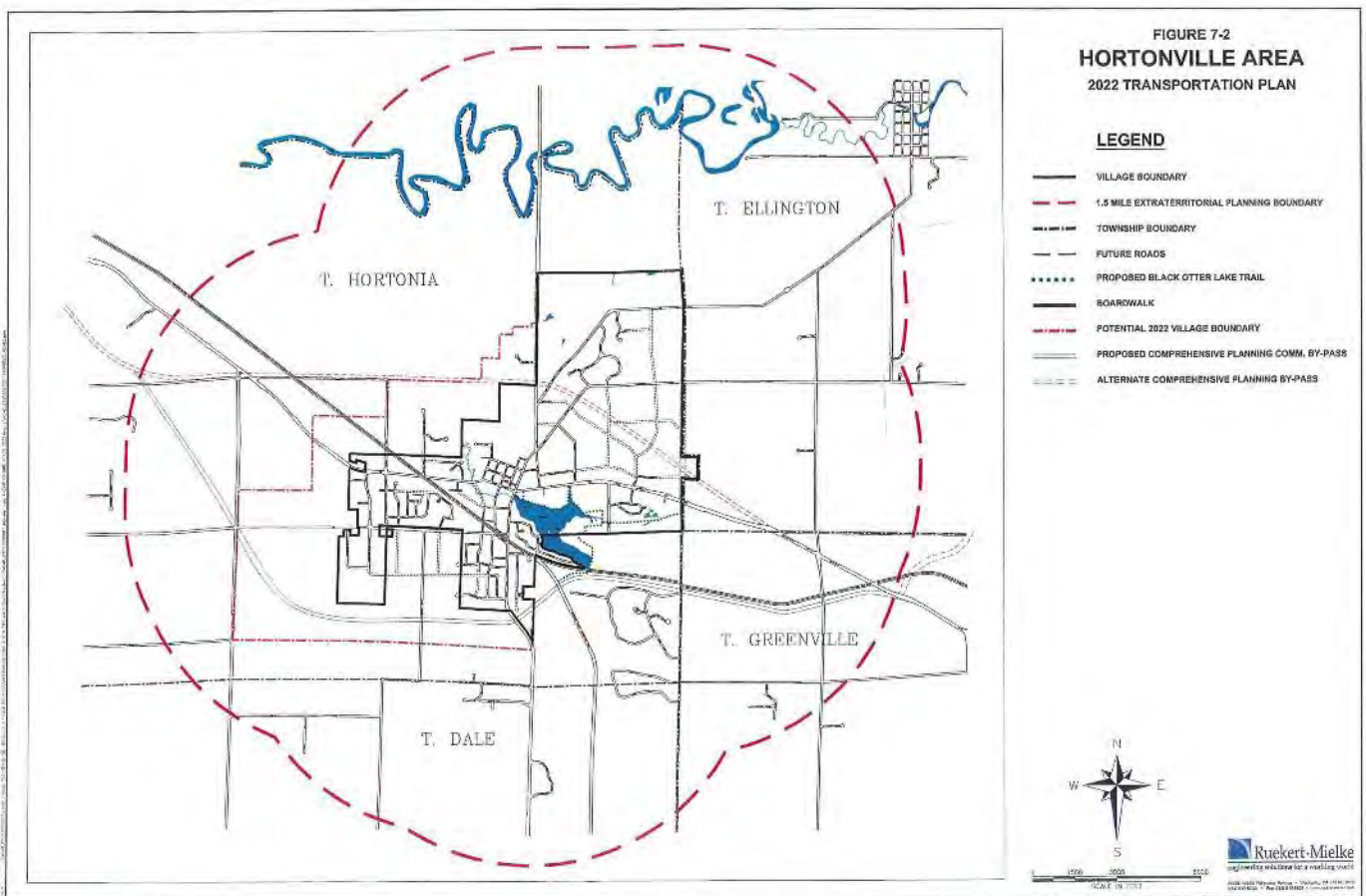
## Background Summary

Expanding the trail system and creating other bicycle/pedestrian connections throughout the community is not an entirely new concept. In fact, the *Village of Hortonville Comprehensive Plan (2003)* offers a good deal of language supportive of these ideas. For example, “development of ... a series of bicycle and pedestrian trails linking residential neighborhoods, commercial areas, institutional uses, and park and recreational areas” is a basic transportation goal listed in that document (page 7). In addition, a proposed Black Otter Lake Trail, “intended to accommodate year-round, multi-modal uses,” is shown on the Future Transportation Plan Map (figure 7-2). Below are other related excerpts from that plan:

- Anticipated timetable for capital improvements (page 59):
  - “Hiking trail construction throughout Village with connection to Wiouwash Trail – 2005-2022”

- “Boardwalk construction through wetlands areas – 2005-2022”

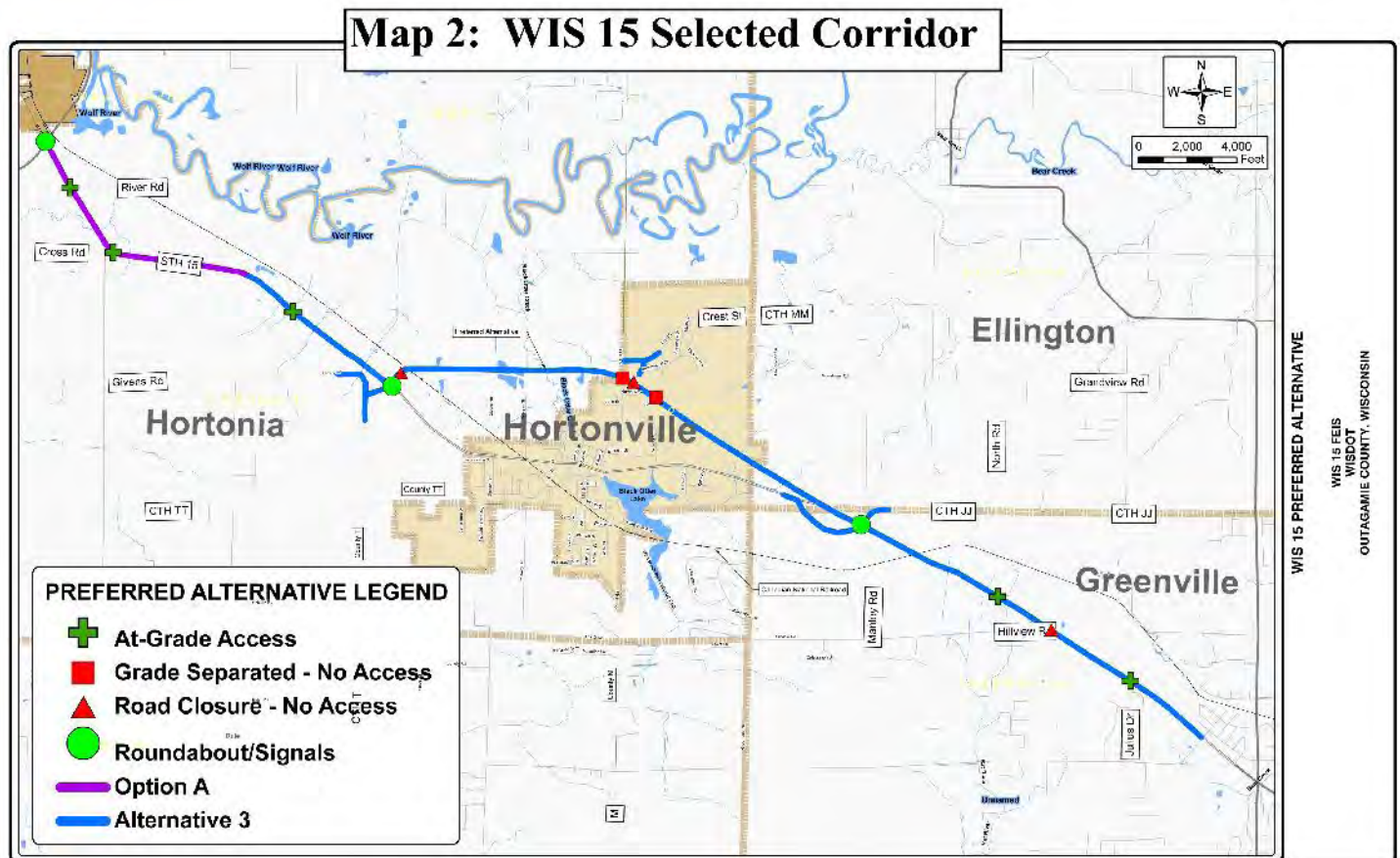
- “... the Wiouwash Trail represents part of Phase I of the *Outagamie County Greenway Plan (1995)*. Under Phase II of the Greenway Plan, the Wiouwash Trail would be connected to the Fox Valley cities via a trail along, or parallel to, the Wisconsin Central Railroad (Canadian and Pacific Railroad). In Phase III of the Greenway Plan, the Wiouwash Trail would be extended north through the Village of Hortonville along CTH MM to the Wolf River and another system of trails... Hortonville would serve as one of the crossroads for several of these trails. This could strengthen the Village’s position as a recreational gateway to the Wolf River and increase its tourism potential. A spillover effect could be enhanced economic revitalization of downtown Hortonville if the Village is able to successfully lure some of the potential tourist traffic into the downtown shops, taverns, and restaurants.” (pages 78-79)



- Transportation objectives and policies (page 86):
  - “The Village will endeavor to provide additional pedestrian/bicycle crossings at major intersections to facilitate safe pedestrian and vehicular access throughout the Village.”
  - “The Village may consider expanding the existing pedestrian and bicycle trail system throughout the Village to connect subdivisions with the downtown area, parks and the schools and may also consider connecting the system to the Wiouwash Trail and developing trails as outlined in the 1995 Outagamie County Greenway Plan. The pedestrian and bicycle system should be built to a minimum width of 8’ to 10’.”

Another notable planning effort is the Wisconsin Department of Transportation’s (WisDOT) “WIS 15 Improvement Project.” Through this process, WisDOT is planning to expand 11 miles of WIS 15, from US 45 in New London to Lily of the Valley Drive in Greenville, to a 4-lane divided highway

bypassing the Village of Hortonville to the north. Roundabouts will be installed at the County T/ Givens Road/WIS 15 intersection and the County JJ/WIS 15 intersection, which will connect the new bypass to existing WIS 15. Other project improvements include bridges carrying WIS 15 over Nash Street, County M and the Canadian National Railroad (west of Hortonville), as well as a 10-foot wide multi-use trail on the south side of WIS 15 connecting the east side of Hortonville to Julius Drive in Greenville. Design work is ongoing, with real estate acquisition scheduled to start in 2014 and construction scheduled for late 2016. More information is available on WisDOT’s project website – <http://www.dot.wisconsin.gov/projects/neregion/15/>. Obviously, this project will have a significant impact on the way people move around in the Hortonville area. With the development of a Hortonville to Greenville trail and Main Street no longer functioning as a state highway, in particular, there will be an opportunity to take on other projects that focus on expanding and enhancing multi-modal connections within the Village.



*Selected Corridor Map for the WIS 15 bypass developed by the Wisconsin Department of Transportation.*



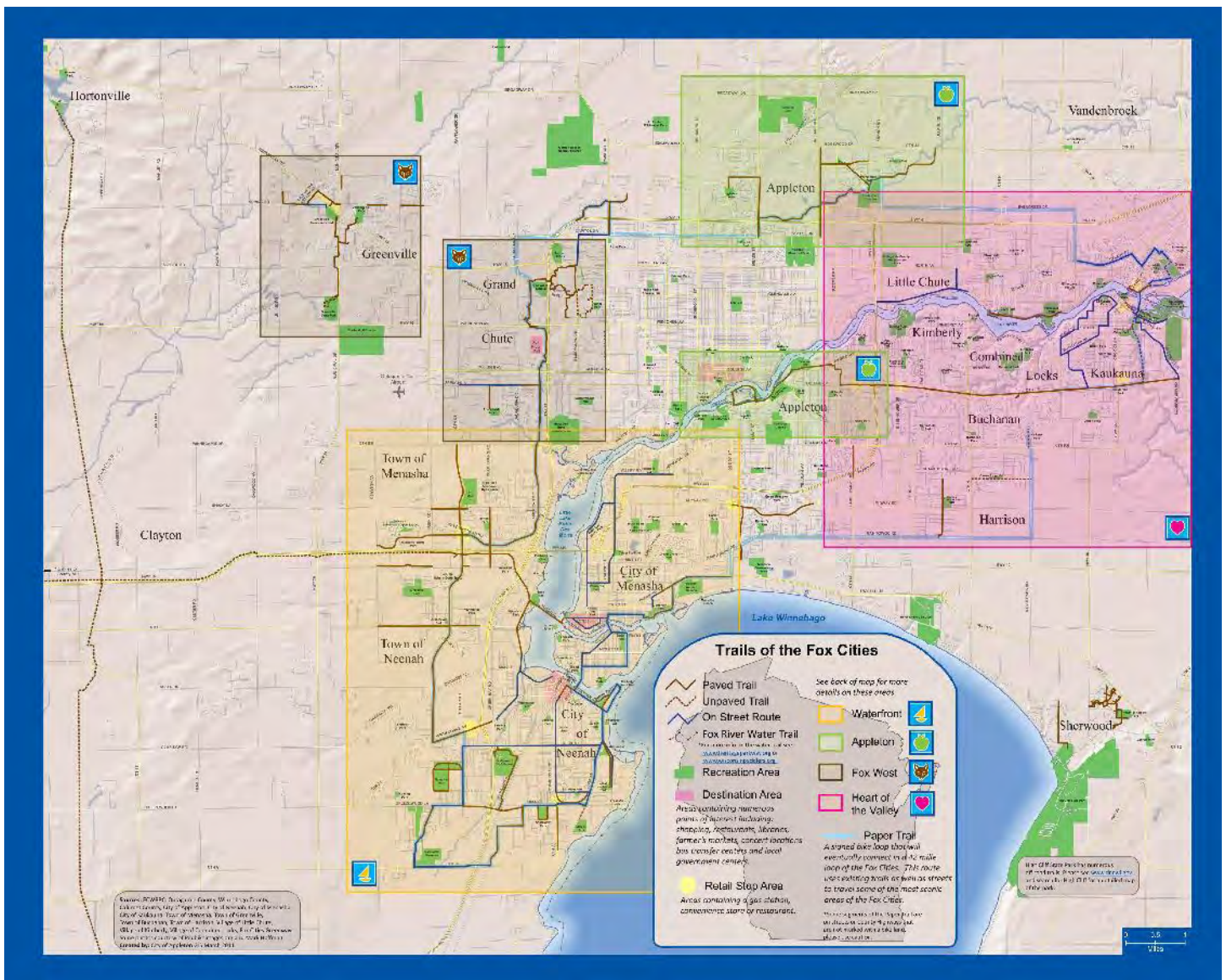
## Regional Context

Over the past several years, multi-modal connections, such as trails and bike routes, have continued to gain momentum in the greater Fox Cities area. Example projects include the CE Trail extending from Appleton to Kaukauna, the Trestle Trail across Little Lake Butte des Morts, and bike lanes on numerous local streets. Many of these projects were made possible -- from planning to funding to implementation -- by the efforts of regional, county, and local agencies. Non-profit organizations like Fox Cities Greenways, Inc. have also played an important role in advocating for and assisting with these projects. As the network of trails and other multi-modal connections continues to develop throughout the region, local

municipalities stand to benefit from linkages to and through their community.

## Connecting Origins & Destinations

The Village of Hortonville is a vibrant community of over 2,700 residents. It is already home to welcoming neighborhoods, successful businesses, several schools, attractive parks, and the only lake in Outagamie County. For the purposes of identifying needed multi-modal connections within the community, these places can be considered “origins” and “destinations,” or locations where people wish to get to/from. This report will focus on six of these places in particular – (1) Wiouwash Trailhead, (2) Alonzo & Black Otter Parks,



Trails of the Fox Cities Map, 2011.



(3) Downtown, (4) East End of Village (Greendale Road), (5) Miller Park, and (6) Industrial Park – and cover various concepts and considerations for forming linkages between them.



### Wiouwash Trail

The Wiouwash State Trail is named for Winnebago, Outagamie, Waupaca and Shawano counties. This crushed limestone trail, built on a former rail corridor, is maintained and operated by these four counties the trail traverses. Two sections of the trail are now complete, with the northern segment traveling 19 miles between Birnamwood and Split Rock in Shawano County and the southern segment open for 22 miles from Hortonville to the City of Oshkosh. The southern segment skirts along the edge of Lake Butte des Morts and continues into the countryside through native tall grass prairies, farmlands, and wildlife areas. This segment also connects with the Friendship Trail at U.S. Highway 10, which leads to the City of Menasha to the east. However, a gap of about 30 miles still separates the northern and southern segments of the Wiouwash Trail. Ongoing efforts are being made to acquire right-of-way or obtain the necessary easements to complete the trail, as is outlined in the Wisconsin Department of Natural Resources' (WDNR) *Wiouwash State Recreation Trail Master Plan* (1995) available here: <http://dnr.wi.gov/topic/lands/>

[masterplanning/documents/MP-PR-WiouwashST-1995.pdf](http://dnr.wi.gov/files/PDF/pubs/pr/PR0472.pdf). Once completed, the trail may eventually run from the Town of Aniwa in Shawano County to downtown Oshkosh.

In Hortonville, the trailhead and parking lot can be accessed from Lakeview Avenue. This is used year-round by residents and visitors alike. Permitted trail activities include walking, bicycling, jogging, horseback riding, and in the winter, snowmobiling, cross-country skiing, and snowshoeing. Leashed pets are also allowed. Given the diversity of these activities, trail users are encouraged to follow good trail etiquette; see WDNR's *Wisconsin Trail Etiquette & Safety Guidelines* for helpful tips: <http://dnr.wi.gov/files/PDF/pubs/pr/PR0472.pdf>.



### Alonzo & Black Otter Parks

Alonzo Park is perhaps the Village's most well-known and well-used recreational facility. This 11.5-acre park was established in 1974 and includes playground equipment, sandboxes, a covered shelter with picnic tables, volleyball pits, horseshoe pits, a nature trail, and restrooms. It is also home to the Hortonville Farmer's Market,



which runs every Wednesday afternoon from June through September. Black Otter Park is located directly next to Alonzo Park. On its 0.3 acres, one can find boat access, a picnic area, and a small shelter. Until recently, Black Otter Park was owned and operated by Outagamie County, but in May 2012, the deed was officially transferred over to the Village of Hortonville. The terms of the agreement require the Village to preserve and maintain the property as a public park.

These two adjacent parks are located on the shores of the 75-acre Black Otter Lake, manmade in 1848. The lake has a maximum depth of nine feet and is drained by Black Otter Creek, which flows into the Wolf River just north of Hortonville. It is the only public access lake in Outagamie County, and because of this, the Black Otter receives substantial recreational use throughout the year, especially by anglers. Fish species in the lake include: bluegill, largemouth bass, black crappie, yellow perch, northern pike, and catfish. In addition to Alonzo & Black Otter Parks, the Village also maintains a public boat launch (Lakeshore Drive), Veterans Memorial Park (E. Main Street), and a handicapped-accessible fishing pier (S. Nash Street) along the shores of the lake.



### Downtown

Downtown Hortonville is located at the “heart” of the community and serves as the Village’s center for commerce, with small offices, retail, banking, and service-oriented businesses concentrated along Main Street. These businesses include traditional taverns, restaurants, and barber/beauty shops, as well as a few small dry good stores. As Hortonville’s beginning point of settlement, many downtown buildings are older, including several of historic significance. For example, the Hortonville Community Hall (312 W. Main Street) has been on the National Register of Historic Places since 1981. The gothic-style Bethlehem Lutheran Church, located at the corner of W. Embarrass Street and N. Nash Street, was built in 1897 (extensive remodeling in 1960) and features a tower nearly 100-feet tall. The Hortonville Public Library and Post Office are other notable places that also draw people downtown. In addition to the businesses and buildings, the area’s traditional look and feel has been enhanced in recent years with decorative lighting, banners, landscaping, and street furniture. These streetscape features have added to the small community charm of downtown and helped make it more pedestrian-friendly.

Downtown also features two other noteworthy community destinations. Veterans Memorial Park, located on Black Otter Lake, includes a gazebo, walking path, historic marker, and peace memorial. The handicapped-accessible fishing pier, adjacent to the S. Nash Street bridge, offers shoreline fishing opportunities and lake views.







### East End of Village (Greendale Road)

The east end of Hortonville serves as one of the gateways into the community. This developing area is home to a number of businesses located along E. Main Street, as well as Pine Grove Estates, a subdivision with an existing network of walking trails looping around its homes and ponds. From here, just four miles separate the Village of Hortonville and the Town of Greenville --a growing community with a population of 10,602. These two municipalities are both within the Hortonville Area School District's boundaries, and their residents often rely on businesses and facilities located in each community to meet everyday needs.



### Miller Park

Miller Park is a 4.8-acre park located along Black Otter Creek, just west of downtown. Notable amenities include playground equipment, a sandbox, tennis courts, a softball diamond, and restrooms. Because of its popularity, reservations for the use of the Miller Park facilities can be made for special events (by contacting the Village Administration Building).



### Industrial Park

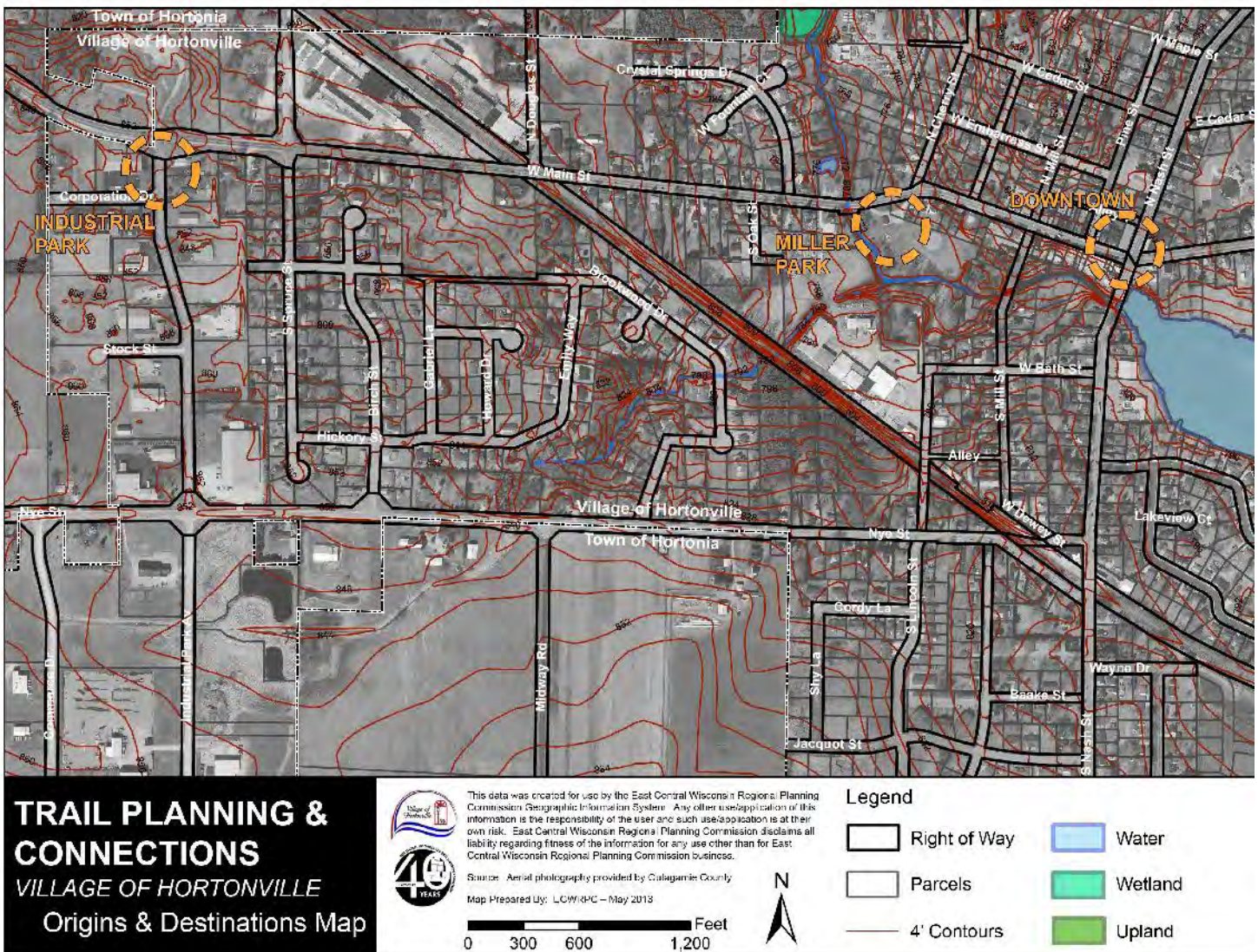
The Hortonville Industrial Park is located on the west side of the Village, immediately south of WIS 15. Here, a number of businesses and employers line Industrial Park Avenue near its intersection with CTH TT / Nye Street. Phase one of the Industrial Park is entirely sold out, but several parcels are still open and available in phase two. Because of its strategic location, reliable infrastructure, and appropriate zoning, the Industrial Park is "shovel ready" for additional business development.



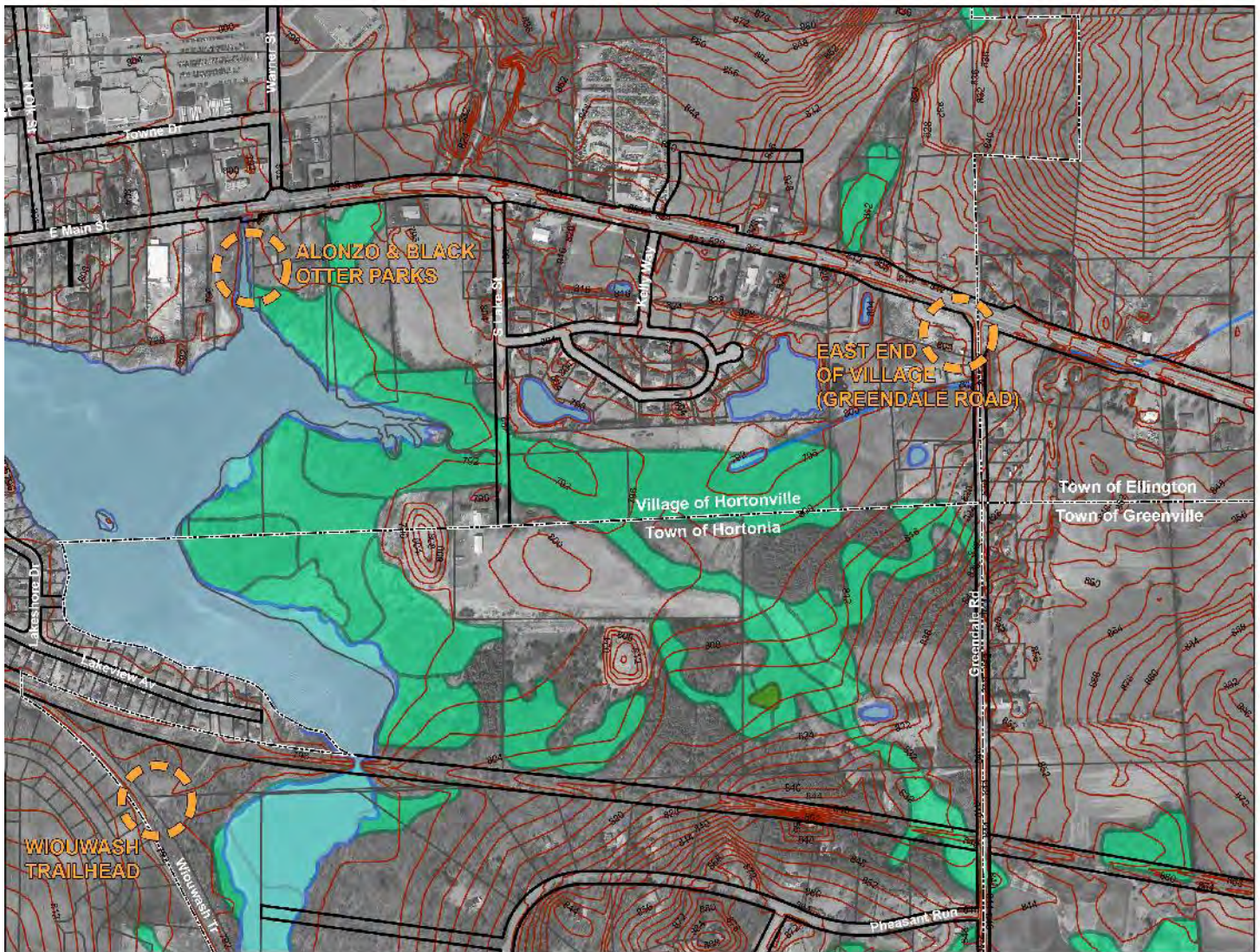
## Project Description

To provide further detail on linking each “origin” and “destination” described above and simplify the various ideas included herein, much of this report will be split up by different connection segments. Segment 01 is a proposed multi-use trail connecting the Wiouwash Trailhead to Alonzo & Black Otter Parks. Segment 02 focuses on a potential on-street connection between the Wiouwash Trailhead and downtown Hortonville. Segment 03 presents a potential on-street connection between Alonzo & Black Otter Parks and the Hortonville Industrial Park. Segment 04 is a proposed sidepath connecting the east end of the Village (Greendale Road) to Alonzo & Black Otter

Parks. Segment 05 features a multi-use trail running behind the businesses on the south side of W. Main Street to form a link between Miller Park and S. Nash Street in downtown. Segment 06 focuses on a shoreline connection between Veterans Memorial Park and the fishing pier downtown. Further information on each of these segments, and the network as a whole, is described in more detail on the following pages.







Continued — Map displaying the location of key “origins” and “destinations.”



# DESIGN CONCEPTS & CONSIDERATIONS

## Overview & Facility Definitions

The concepts included in this section lay out a plan for expanding the system of trails and other multi-modal facilities in Hortonville. Information on each segment will be organized in a similar manner and touch on the following topics – site description, design concept, land access, and trail use. These ideas build upon previous and ongoing planning efforts and incorporate input provided by Village staff and Village Board members. The full trail/connection network could feature a variety of potential improvements, including multi-use trails, bike lanes, and signed travel routes. Brief definitions of each facility type are found below.



### Multi-Use Trails

Multi-use trails are off-street facilities often designed to support a wide variety of non-motorized travelers – bicyclists, in-line skaters, roller skaters, wheelchair users, walkers, runners, people with baby strollers, or people walking dogs. However, some multi-use trails are also open to snowmobile use during the winter. Regardless, they serve as a complementary extension of the roadway network. Multi-use trails may be paved or

unpaved, but should offer a smooth surface for users. They are commonly designed for two-way travel (typical width of 10-feet) and comply with ADA requirements. A multi-use trail may be in its own right-of-way, such as the Wiouwash Trail, or it may share a right-of-way with a street or highway. In either case, careful design at each street or railroad crossing is necessary to increase user safety. (Modified from the WisDOT's *Wisconsin Bicycle Facility Design Handbook* available here: <http://www.dot.wisconsin.gov/projects/state/docs/bike-facility.pdf>).



### Bike Lanes

A bike lane is a portion of the roadway designated for exclusive or preferential use by bicyclists. Bike lanes are always one-way facilities, which are separated from vehicle travel lanes and identified with striping, pavement markings, and signing. On two-way streets, a one-way bike lane should be provided on each side. Bike lanes are the preferred bicycle facility on urban and suburban roadways (i.e. collector and arterial streets), where higher traffic volumes and/or speeds warrant greater separation of bicyclists and motor vehicles. However, they are not utilized as often on low-volume, residential streets. On some streets, bike lanes will fit within the current street width – sometimes as a trade-off with existing vehicle travel lanes or on-street parking. On others, bike lanes will require the street to be widened to accommodate the lane. (Modified from WisDOT's *Wisconsin Bicycle Facility Design Handbook*).

### Signed Bike Routes & Wayfinding

Bike routes are streets and roadways that are identified as bicycle routes by signage. These routes may be on low traffic streets where cyclists and motor vehicles can share the same space. They may also be identified on busier roads that



provide other facilities, such as bike lanes. All bike routes should include wayfinding signage that helps orient users and clearly identifies direction and distance to nearby destinations, such as downtown, schools, and parks. In addition, the wayfinding signage should raise awareness of new multi-modal facilities as the network is being developed and encourage residents to try walking, biking, etc. to different destinations around the community. (Source: *Shawano County Bicycle and Pedestrian Master Plan, 2013*).

### Bike Parking

A component not to be overlooked in any community is the provision for adequate bicycle parking destinations. Investments in bicycle parking will eliminate the clutter, pedestrian hazards, and tree damage from randomly parked bicycles, as well as let people know that they and their bikes are welcome. The most important considerations to ensure successful bicycle parking are a good rack and a good location. Well-designed, short-term bicycle parking facilities allow a cyclist to quickly and easily lock up the bike frame and one wheel to the rack. When choosing a site, consider the following: racks must be convenient, visible, accessible, and should not interfere with other uses. (Modified from the Iowa Department of Transportation's *Iowa Trails 2000: Connecting People and Trails* handbook available here: [http://www.iowadot.gov/iowabikes/trails/web-pdf/Bike-Ped/connecting\\_people\\_trails\\_handbook.pdf](http://www.iowadot.gov/iowabikes/trails/web-pdf/Bike-Ped/connecting_people_trails_handbook.pdf)).



### Sharrows

Shared lane pavement markings (or “sharrows”) are bicycle symbols carefully placed to guide bicyclists to the best place to ride on the road, avoid car doors and remind drivers to share the road with cyclists. Unlike bicycle lanes, sharrows do not designate a particular part of the street for the exclusive use of bicyclists. They are simply a marking to guide bicyclists to the best place to ride and help motorists expect to see and share the lane with bicyclists. (Source: Seattle Department of Transportation's website: <http://www.seattle.gov/transportation/bikefacilities.htm>).



## Segment 01: Wiouwash Trail to Alonzo & Black Otter Parks

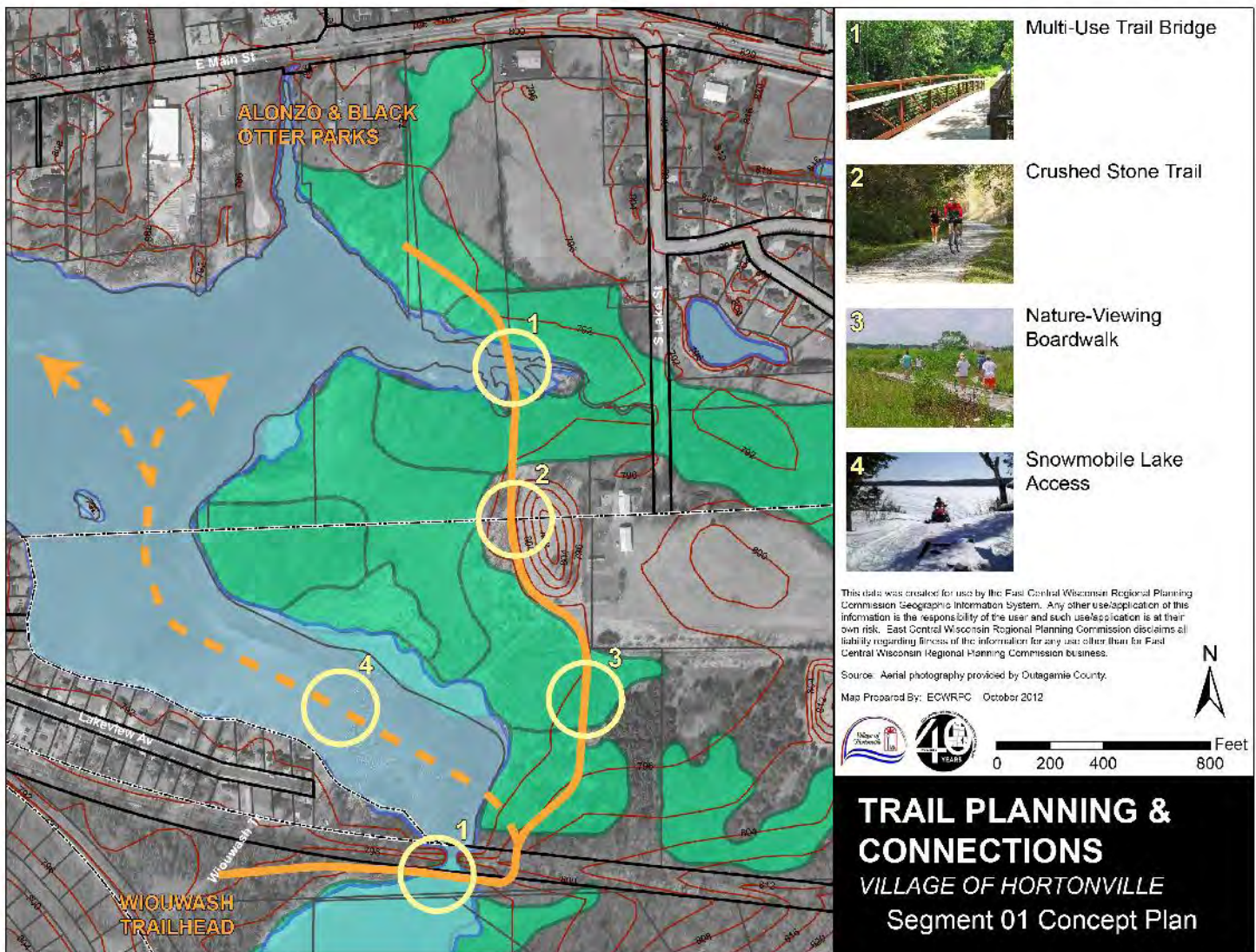
### Site Description

The Village places a high priority on this particular segment. The area included in Segment 01 is located to the south and east of Black Otter Lake, with portions in the Village of Hortonville and the Town of Horton. Here, wetlands are found along the undeveloped eastern shore of the lake. Much of this land is forested with water-tolerant species such as tamarack, white cedar, willow, elm, tag alder, and dogwood. In addition to providing rich habitats that support wildlife, fish, and game communities, these wetlands also filter sediment and nutrient load from surface waters (see page 39 for Generalized Soils Map). A variety of waterfowl

find feeding and nesting areas in the lake, especially the bay upstream of the railroad bridge where there is little disturbance by boaters. (Source: *Black Otter Lake District Adaptive Lake Management Plan, 2008*). Although Segment 01 features mostly natural, undeveloped land, it also includes the Canadian National Railroad, which typically runs trains through Hortonville a few times per week.

### Design

As is shown in the concept plan below, Segment 01 is a proposed multi-use trail connecting the Wiouwash Trailhead to Alonzo & Black Otter Parks. It would feature a crushed stone surface over several stretches, a nature-viewing boardwalk through wetland areas, and two trail bridges to

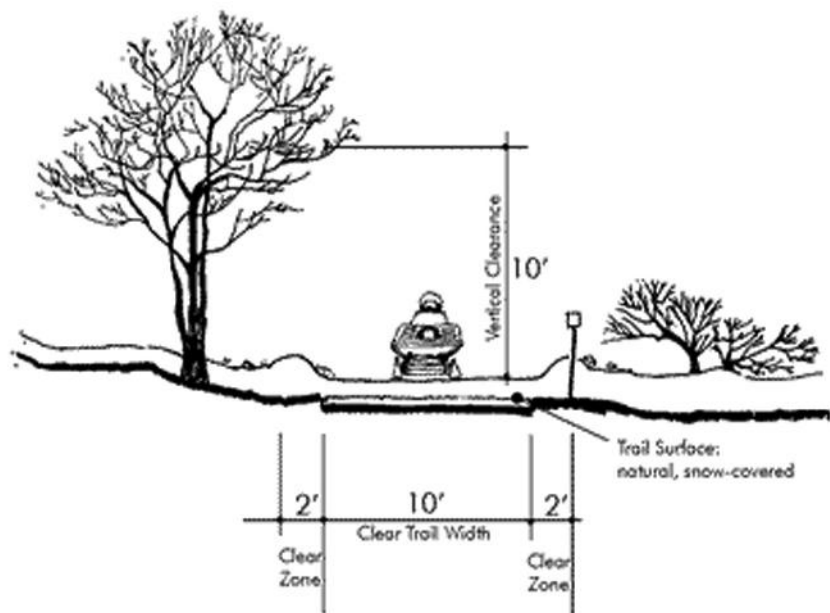


cross open water portions of Black Otter Lake. The full length would be approximately 3,915 feet (or .74 miles). During winter months, a southern section of the trail would also provide snowmobile access to the lake. Special permission may be required to cross the Canadian National Railroad, and Outagamie County has adopted a Flood Plain Zoning Ordinance with certain development constraints for designated flood hazard areas. The following are some general guidelines/considerations for Segment 01; however, project engineering will specify exact design requirements (modified from *Iowa Trails 2000*).

- Accessible facilities should be developed that meet the needs of older adults and people with disabilities (as possible while maintaining the essential character of the resource).
- Recommended trail width of 10 feet.
- Maintain a minimum 2-foot “clear zone” on each side of the trail.
- Trail used by snowmobiles should maintain at least 10 feet of vertical clearance above the average snow level to accommodate grooming equipment.
- The trail surface should be smooth and free of tread obstacles.
- Placement of snowmobile trails on asphalt surfaces should be avoided, as studs will cause damage to the asphalt.
- Trail should be well drained; standing water on the trail will decrease its life and quality.
- The distance a user can see along the trail

should never be less than the distance it would take that user to stop.

- Recommended grade no steeper than 5 percent.
- At railroad crossing, sight lines are important – a trail user needs to be able to judge his or her ability to cross safely.
- Trail should cross railroad at right angles, if possible. Also, a ramp leading up to the railroad tie should be developed with a level landing on either side.
- Signage should be based on the FHWA's *Manual on Uniform Traffic Control Devices*, and be placed outside the recommended clear zones for both trails and roadways.
- Recommended width of independent trail bridge: trail width plus recommended clear zones (to reduce cost, clear zones may be reduced for trail bridges).
- There should be a flush transition between the trail and the trail bridge.
- Design loading: heaviest vehicle that will cross the bridge, including maintenance vehicles.
- For trail bridges over watercourses, 42-inch fencing on both sides of trail.
- Reduce the amount of fill in the floodplain by setting bridge abutments as low as possible.
- Avoid setting bridge piers directly in the watercourse, as this could disrupt flow and trap debris.
- The planks of the boardwalk should be perpendicular to the dominant direction of travel to prevent bicycle tires and wheelchair wheels



Conceptual cross section of potential multi-use trail -- used seasonally for snowmobiles -- along Segment 01 (modified from *Iowa Trails 2000*).

from becoming stuck in the openings.

- Wood slats (boardwalk) should be placed as close together as possible while still allowing for drainage between them.
- Curves and angles should be very slight on boardwalk, as traction on wood is reduced, especially when wet.
- Because some wetlands have fluctuating water levels, the trail surface should be located above the normal high water line of the wetland.

### Access

In order to carry out the concepts included here, access to land – with various owners – will need to be granted. This could be accomplished in several ways, including direct acquisition (purchase) and/or establishing easements. Although the exact location and alignment of Segment 01 could vary upon implementation, below is a table of current property owners included in the concept plan.

| Property Owner             | Approximate Distance | Percent of Total |
|----------------------------|----------------------|------------------|
| Outagamie County           | 1,095 feet           | 28%              |
| Canadian National Railroad | 115 feet             | 3%               |
| Patrick & Margie Cousineau | 2,190 feet           | 56%              |
| Richard & Larue Hunt       | 445 feet             | 11%              |
| Village of Hortonville     | 70 feet              | 2%               |
| <b>TOTAL</b>               | <b>3,915 feet</b>    | <b>100%</b>      |

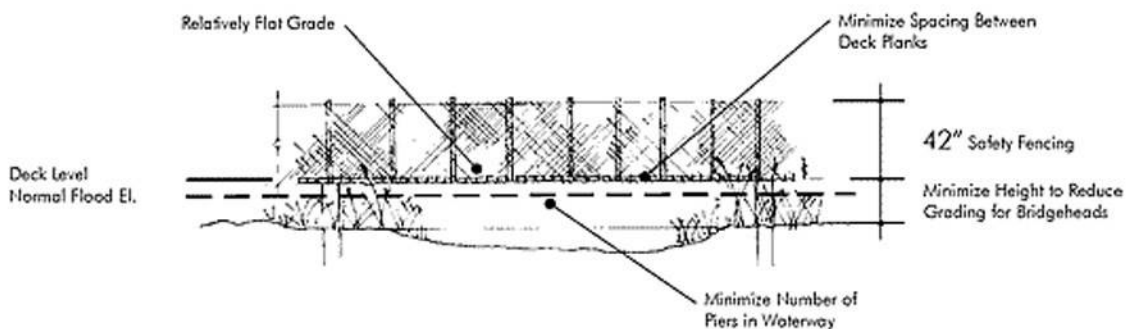
### Uses

The concept plan for Segment 01 invites several trail user groups to utilize the trail, as listed below. This broad spectrum of trail users has the potential for conflict because of their varying trail needs and styles of recreation. Education of the various trail users and promoting responsible behavior will help minimize trail conflicts. For example, trail etiquette standards can be publicized through signage and other educational materials.

- Hikers
- Walkers
- Joggers
- Bicyclists
- Families, Schools, and Environmental Education Groups
- Cross-Country Skiers (seasonal)
- Snowshoers (seasonal)
- Snowmobilers (seasonal and southern section only)

### Cost

A list of general costs for elements typically included in trail projects is shown on page 32. This may serve as a helpful resource as features included in the Segment 01 concept plan, such as crushed stone trail, boardwalk, and bridges, are evaluated further.



*Conceptual sketch of potential nature-viewing boardwalk along Segment 01 (modified from Iowa Trails 2000).*



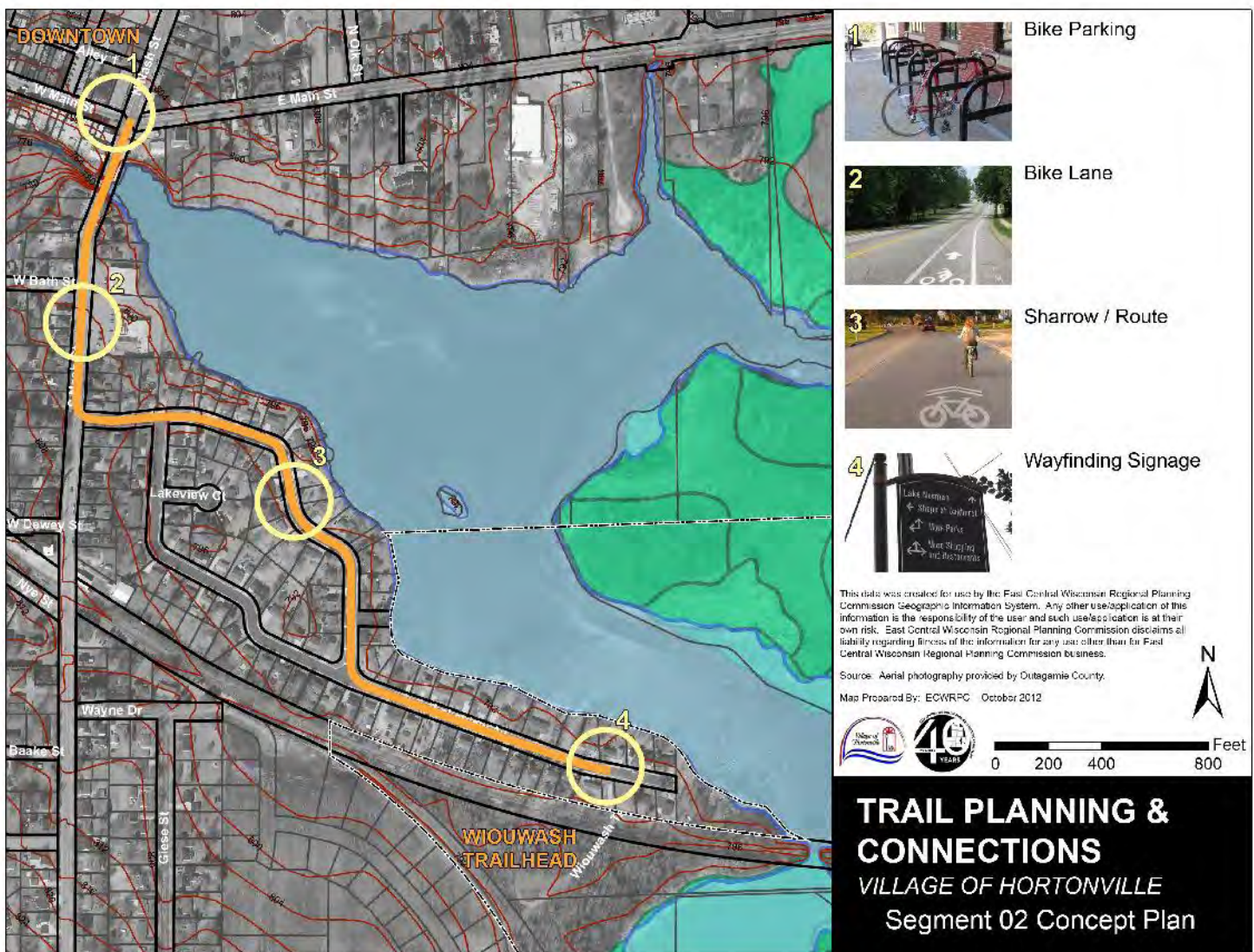
## Segment 02: Wiouwash Trail to Downtown

### Site Description

The area included in Segment 02 is located entirely within the public right-of-way, as it travels along three existing streets on the south side of the Village of Hortonville. Lakeview Avenue and Lakeshore Drive are fronted primarily by single-family homes, and both streets have an existing curb-to-curb width of 38 feet. CTH M / S. Nash Street is lined with a mix residential and commercial properties, especially as it approaches downtown. Its curb-to-curb width is 44 feet, and as the Village's major north-south thoroughfare, it had an average daily traffic count of 4,500 vehicles in 2010.

### Design

As is shown in the concept plan below, Segment 02 focuses on a potential on-street connection between the Wiouwash Trailhead and downtown Hortonville. It would feature a signed bike route and/or sharrow along Lakeview Avenue and Lakeshore Drive, as well as bike lanes on CTH M / S. Nash Street, which may require collaboration with the Outagamie County Highway Department. On-street facilities such as these may also involve a trade-off between lane widths, parking, etc. The full segment length would be approximately 3,960 feet (or .75 miles). Increasing the amount of well-located bike parking in the downtown area is another component of the Segment 02 concept. The following are some general guidelines/considerations for Segment 02; however, project

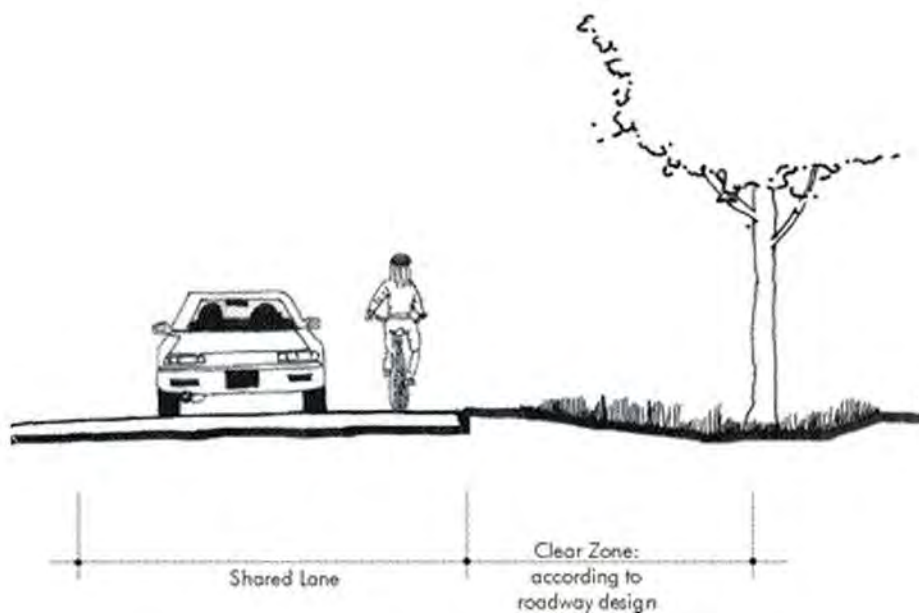


Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for Segment 02 and the key features included in its concept plan.

engineering will specify exact design requirements (modified from the National Association of City Transportation Officials' (NACTO) *Urban Bikeway Design Guide* available here: <http://nacto.org/cities-for-cycling/design-guide/> and WisDOT's *Wisconsin Bicycle Facility Design Handbook*).

- Word and/or symbol and arrow pavement markings shall be used to define the bike lane and designate that portion of the street for preferential use by bicyclists.
- On two-way streets, bike lanes should always carry traffic in the same direction as the adjacent motor vehicle flow.
- Desirable bike lane width adjacent to a curb face is 6 feet. If illegal parking in bike lanes is a concern, 5-foot wide bike lanes may be preferred.
- When placed adjacent to a parking lane, the desirable reach from the curb face to the edge of the bike lane (including the parking lane and bike lane) is 14.5 feet; the absolute minimum reach is 12 feet. A bike lane next to a parking lane shall be at least 5 feet wide.
- A solid white lane line marking shall be used to separate motor vehicle travel lanes from the bike lane. Most jurisdictions use a 6 to 8 inch line.
- In a bike lane, gutter seams, drainage inlets, and utility covers should be flush with the ground and oriented to prevent conflicts with bicycle tires.

- Where space is limited due to multiple turn lanes, approaching bike lanes may be dropped in advance of an intersection. Here, a cyclist assumes proper lane positioning and picks up the designated bike lane on the other side of the intersection.
- Signage and pavement markings should be based on the FHWA's *Manual on Uniform Traffic Control Devices (MUTCD)*.
- Bike Route signs should be placed in advance of all turns or decision points. They should include destinations, directional arrows, and distance. Confirmation signs should be placed every 2 to 3 blocks along on-street routes.
- The Shared Lane Marking in use within the U.S. is the bike-and-chevron "sharrow."
- Sharrows shall not be used on shoulders, in designated bike lanes, or to designate bicycle detection at signalized intersections.
- Frequent, visible placement of sharrows is essential. Along busier streets, they should be placed more frequently (50 to 100 feet), and along low-volume routes they can be placed less frequently (up to 250 feet or more), but staggered by direction.
- When an on-street parking lane is present, lateral placement of sharrows should help avoid the "door zone" and encourage safe passing behavior. MUTCD guidance recommends placement at least 11 feet from the curb face.
- If on-street vehicle parking is not present, sharrows should be placed far enough from the



*Conceptual cross section of potential shared lane along Segment 02 (modified from Iowa Trails 2000).*

curb to direct bicyclists away from gutters, seams, and other obstacles. MUTCD guidance recommends placement at least 4 feet from the curb face.

- On streets with posted 25 mph speeds or slower, preferred sharrow placement is in the center of the travel lane to minimize wear and encourage bicyclists to occupy the full travel lane.
- On streets with posted 35 mph speeds or faster and traffic volumes higher than 3,000 vehicles per day, sharrows are not a preferred treatment. Here, other bikeway types are preferred.

### Access

Unlike some of the other connections featured in this document, no land acquisition or easements will be needed to move forward with Segment 02. In order to carry out the concepts included here,

however, some existing public right-of-way would be repurposed to accommodate other users. Although the exact location and alignment of Segment 02 could vary upon implementation, below is a table of the current right-of-way included in the concept plan.

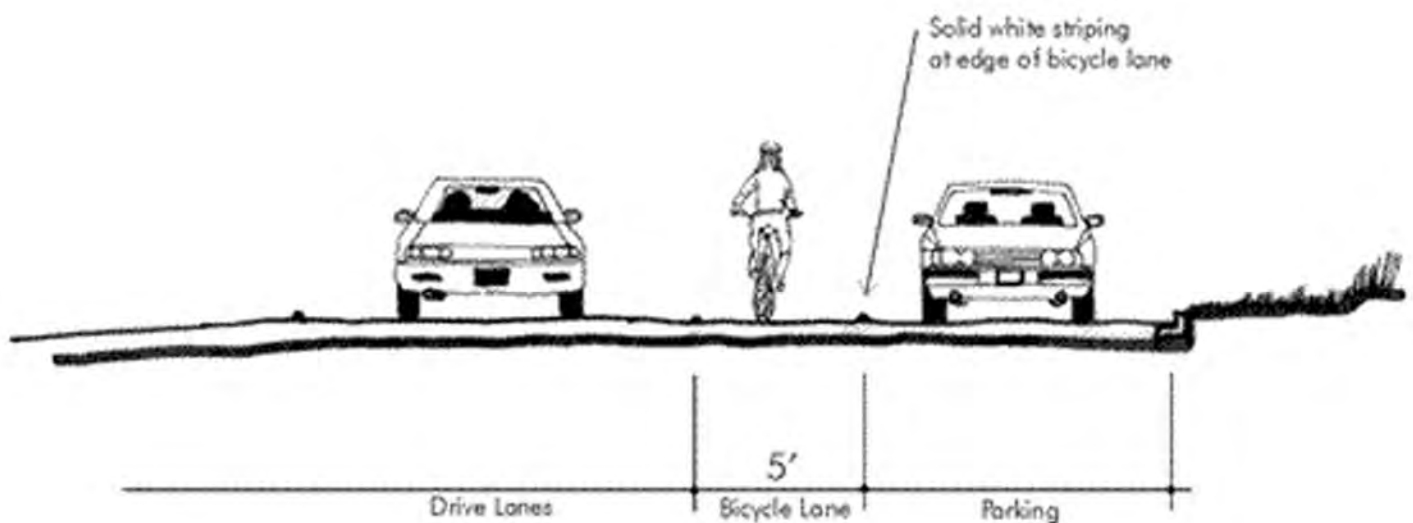
### Uses

Bicyclists will be the primary users of the facilities included in the concept plan for Segment 02. Since these are entirely on-street accommodations, however, bicyclists and motorists will be expected to interact. Educating both groups on proper usage will be important, especially since these facilities are relatively new to the area. Appropriate signage can help clarify expectations for motorists and bicyclists alike. In addition, signage that includes wayfinding can benefit other roadway users, such as pedestrians.

### Cost

A list of general costs for elements typically included in on-street facility projects is shown on page 32. This may serve as a helpful resource as features included in the Segment 02 concept plan, such as shared lane pavement markings, bike lane striping, route signs, and bike racks, are evaluated further.

| Street Name            | Approximate Distance | Percent of Total |
|------------------------|----------------------|------------------|
| Lakeview Avenue        | 1,035 feet           | 26%              |
| Lakeshore Drive        | 1,750 feet           | 44%              |
| CTH M / S. Nash Street | 1,175 feet           | 30%              |
| <b>TOTAL</b>           | <b>3,960 feet</b>    | <b>100%</b>      |



Conceptual cross section of potential bike lane along Segment 02 (modified from Iowa Trails 2000).



## Segment 03: Alonzo & Black Otter Parks to Industrial Park

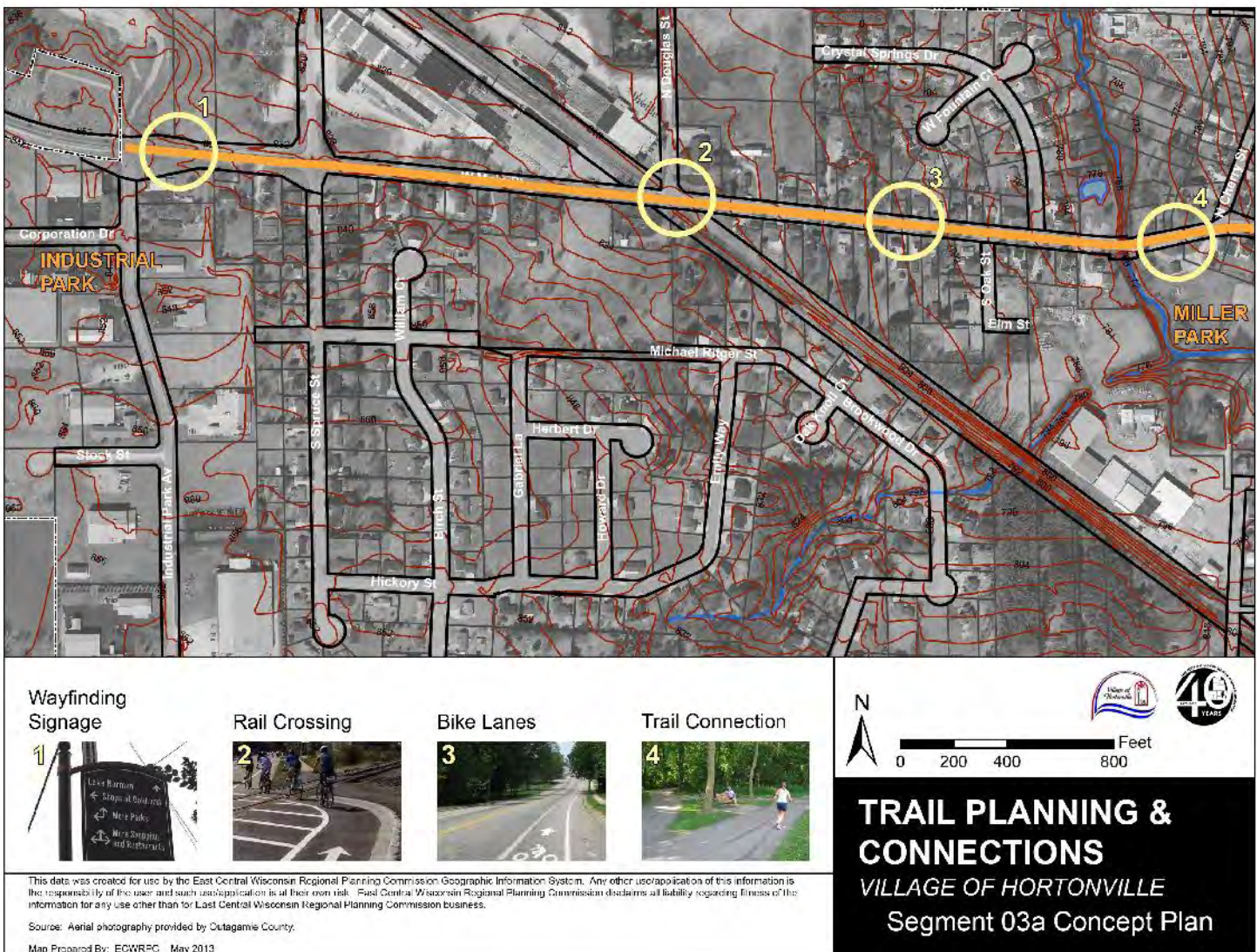
### Site Description

The area included in Segment 03 is located entirely within the public right-of-way, as it travels along Main Street (currently WIS 15) through the middle of the Village. As the Village's primary east-west corridor, it had an average daily traffic count of 10,700 vehicles in 2010. This stretch of E. Main Street is fronted by a mix of residential, commercial, institutional, and industrial properties, and its curb-to-curb width is 50 feet. Through downtown, W. Main Street is lined mostly with small offices, retail, and service-oriented businesses, such as restaurants and taverns. On-street parking is also found throughout this stretch

of Main Street, and its curb-to-curb width is 52.5 feet. Further west, the street crosses Black Otter Creek, the Canadian National Railroad, and passes a mix of residential and industrial properties. Here, the street transitions to a rural cross section with shoulders and ditches.

### Design

As is shown in the concept plan below, Segment 03 focuses on a potential on-street connection between Alonzo & Black Otter Parks and the Hortonville Industrial Park. It would feature bike lanes along most of this stretch of WIS 15 / Main Street, as well as a wide paved shoulder on the far west end of the segment. Typically, on-street facilities such as these may involve a trade-off between lane widths, parking, etc. As the roadway



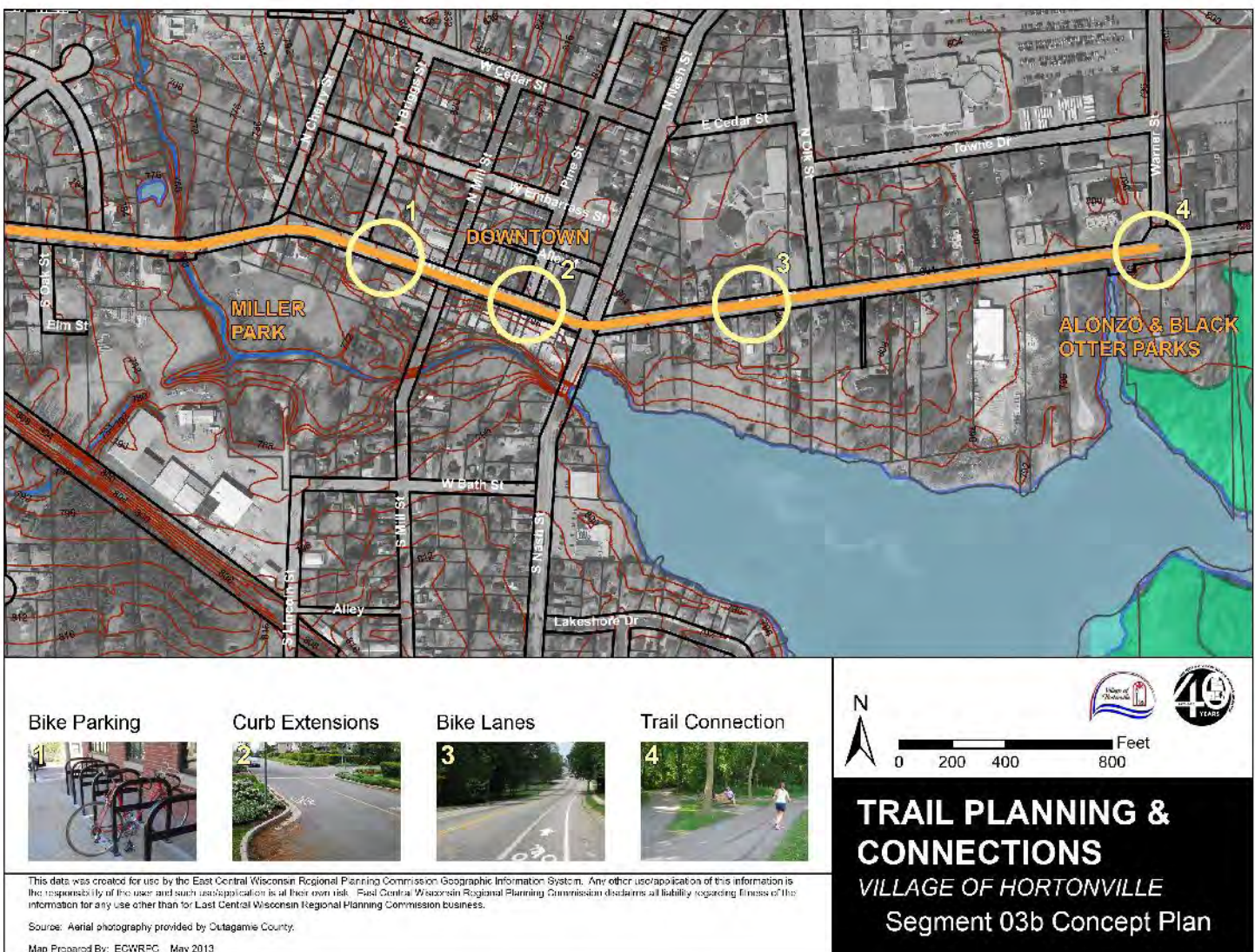
Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for Segment 03 and the key features included in its concept plan.



transfers to a county highway with the WIS 15 bypass project, collaboration with the Outagamie County Highway Department will be necessary. Special permission may also be required to include facilities across the Canadian National Railroad. The full segment length would be approximately 7,510 feet (or 1.42 miles). Adding curb extensions, which should calm traffic and reduce pedestrian crossing distances, and increasing the amount of well-located bike parking in the downtown area are other components of the Segment 03 concept. Initial design work has already been completed for portions of this segment (see image on page 21 more information). The following are some general guidelines/considerations for Segment 03; however, project engineering will specify exact design requirements (modified from the Federal Highway Administration's (FHWA) *Pedestrian*

*Safety Guide and Countermeasure Selection System (2004)* available here: <http://www.walkinginfo.org/pedsafe/about.cfm>, *Iowa Trails 2000*, National Association of City Transportation Officials' (NACTO) *Urban Bikeway Design Guide*, and WisDOT's *Wisconsin Bicycle Facility Design Handbook*).

- Word and/or symbol and arrow pavement markings shall be used to define the bike lane and designate that portion of the street for preferential use by bicyclists.
- On two-way streets, bike lanes should always carry traffic in the same direction as the adjacent motor vehicle flow.
- Desirable bike lane width adjacent to a curb face is 6 feet. If illegal parking in bike lanes is a concern, 5-foot wide bike lanes may be preferred.



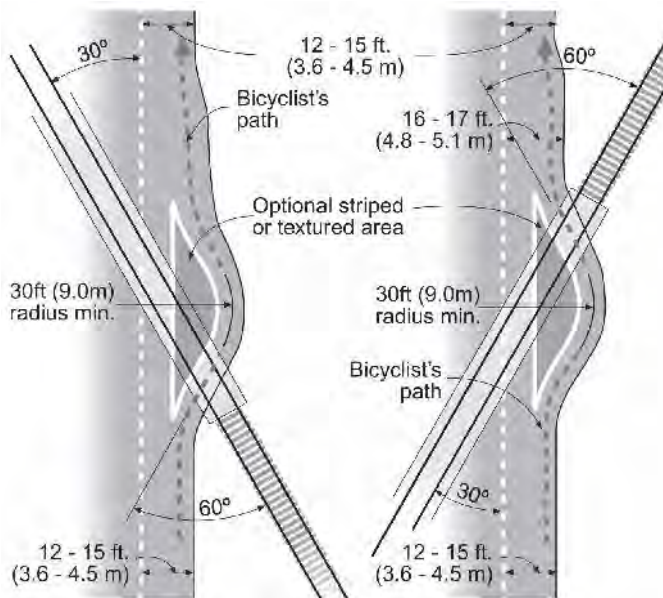
*Continued — Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for Segment 03 and the key features included in its concept plan.*



- When placed adjacent to a parking lane, the desirable reach from the curb face to the edge of the bike lane (including the parking lane and bike lane) is 14.5 feet; the absolute minimum reach is 12 feet. A bike lane next to a parking lane shall be at least 5 feet wide.
- A solid white lane line marking shall be used to separate motor vehicle travel lanes from the bike lane. Most jurisdictions use a 6 to 8 inch line.
- In a bike lane, gutter seams, drainage inlets, and utility covers should be flush with the ground and oriented to prevent conflicts with bicycle tires.
- Where space is limited due to multiple turn lanes, approaching bike lanes may be dropped in advance of an intersection. Here, a cyclist assumes proper lane positioning and picks up the designated bike lane on the other side of the intersection.
- Signage and pavement markings should be based on the FHWA's *Manual on Uniform Traffic Control Devices (MUTCD)*.
- Bike Route signs should be placed in advance of all turns or decision points. They should include destinations, directional arrows, and distance. Confirmation signs should be placed every 2 to 3 blocks along on-street routes.
- At railroad crossings, sight lines are important – a user needs to be able to judge his or her ability to cross safely.
- Bike facilities should cross railroad at 90-degree angles, if possible. When the crossing

is not at this ideal angle and where right-of-way allows, another approach is to flair the roadway, bike lane, or path to allow for a more perpendicular approach.

- The width of the gap between the side of the rail and the roadway surface should be minimized – fillers made of rubber or polymer can help reduce this gap.
- Railroad crossings should be regularly maintained to prevent unevenness and poor conditions.
- In order to be designated or marked as bikeways, paved shoulders should meet these width guidelines – 4 feet minimum or 5 feet from a barrier or railing – and be free from obstructions or treatments such as rumble strips.
- Curb extensions are only appropriate where there is an on-street parking lane. They should not extend into travel lanes, bike lanes, or shoulders.
- Curb extensions should reduce the pedestrian crossing distance and time, visually and physically narrow the roadway, and improve the ability of pedestrians and motorists to see each other.
- Where intersections are used by significant numbers of trucks or buses, the curb extensions need to be designed to accommodate them. However, those vehicles should not be going at high speeds and most can make a tight turn at slow speeds – curb radii should be as tight as practicable.



Left: Sample railroad crossing designs (from WisDOT's *Bicycle Facility Design Handbook*). Above: Photo of bike lane railroad crossing on E. Newberry Street in Appleton, WI.

## Access

Unlike some of the other connections featured in this document, no land acquisition or easements will be needed to move forward with Segment 03. In order to carry out the concepts included here, however, some existing public right-of-way would be repurposed to accommodate additional users. Although the exact location and alignment of Segment 03 could vary upon implementation, below is a table of the current right-of-way included in the concept plan.

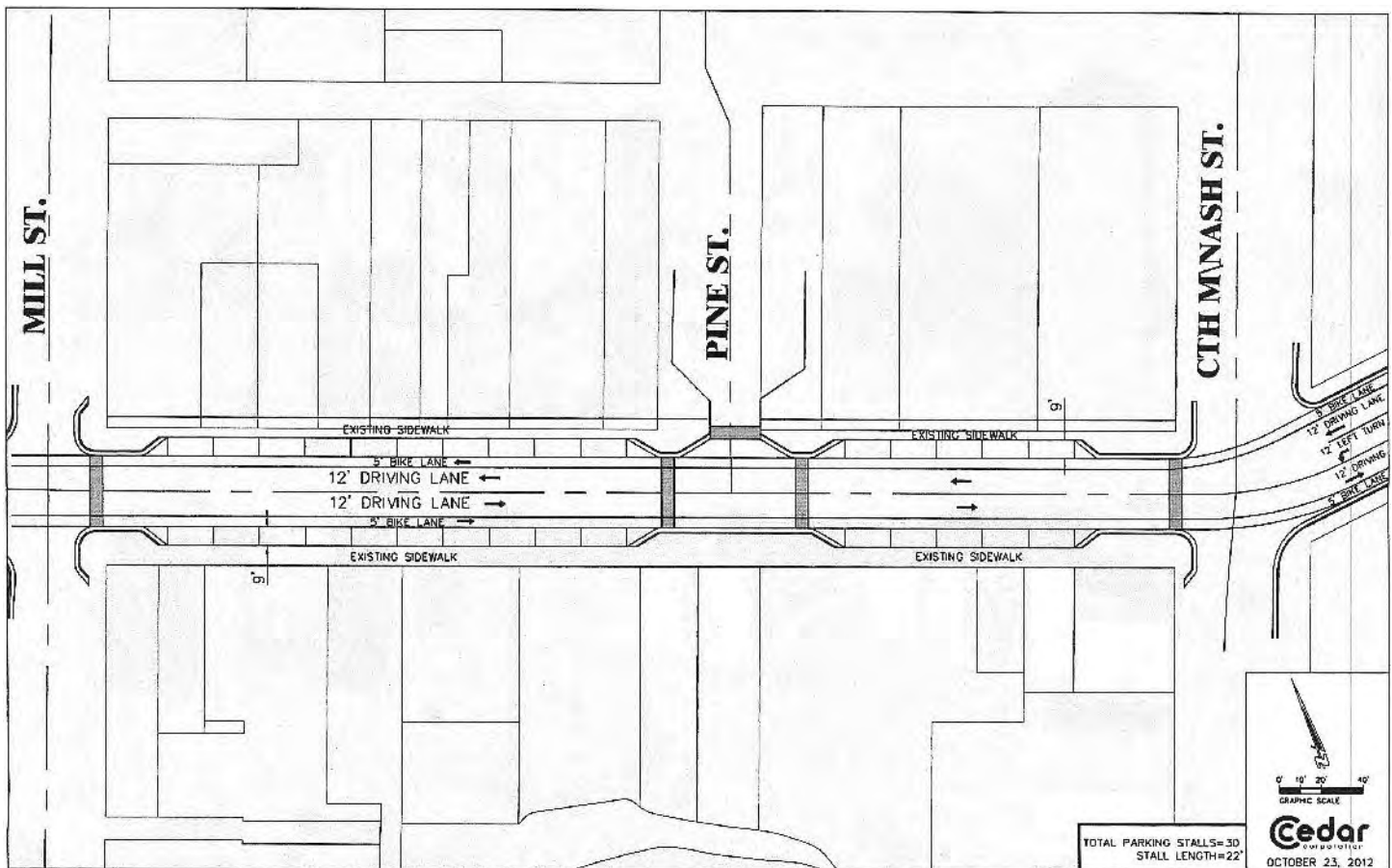
| Street Name             | Approximate Distance | Percent of Total |
|-------------------------|----------------------|------------------|
| WIS 15 / W. Main Street | 5,350 feet           | 71%              |
| WIS 15 / E. Main Street | 2,160 feet           | 29%              |
| <b>TOTAL</b>            | <b>7,510 feet</b>    | <b>100%</b>      |

## Uses

Bicyclists will be the primary users of the facilities included in the concept plan for Segment 03, but pedestrians will also benefit from the curb extensions downtown and the wide paved shoulders where sidewalks do not currently exist. Since these are largely on-street accommodations, pedestrians, bicyclists, and motorists will need to interact. Educating each group on proper usage will be important, especially since some of these facilities are relatively new to the area. Appropriate signage can help clarify expectations and offer wayfinding assistance for all roadway users.

## Cost

A list of general costs for elements typically included in on-street facility projects is shown on page 32. This may serve as a helpful resource as features included in the Segment 03 concept plan, such as bike lane striping, pavement markings, signage, and curb extensions, are evaluated further.



Conceptual site plan of potential bike lanes and curb extensions in downtown along Segment 03 (prepared by Cedar Corporation for Village of Hortonville).



## Segment 04: East End of Village (Greendale Road) to Alonzo & Black Otter Parks

### Site Description

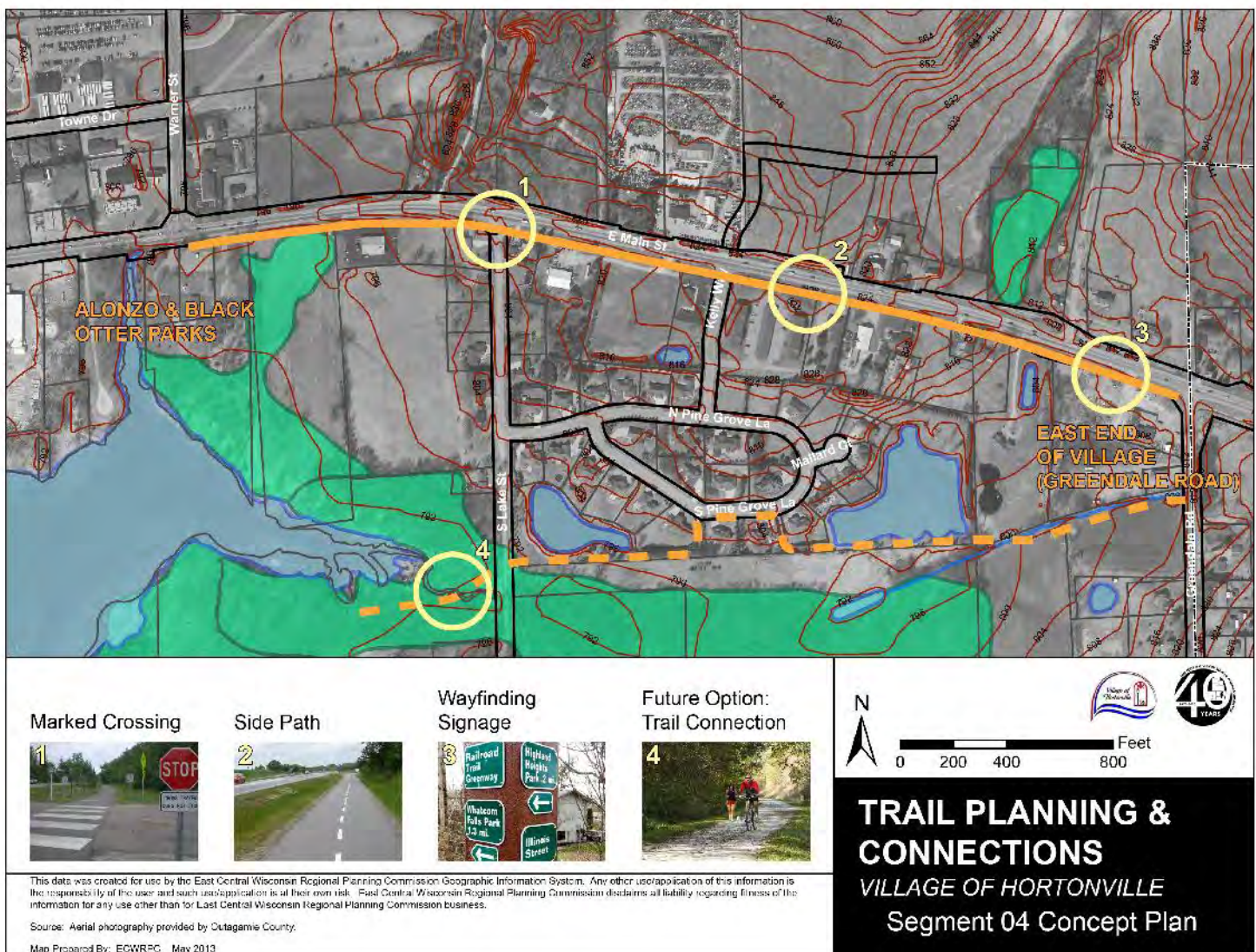
The area included in Segment 04 is located entirely within the public right-of-way, as it travels parallel to E. Main Street (currently WIS 15) to the Village's eastern boundary at Greendale Road. From here, it is another four miles to the Town of Greenville's trails. As Hortonville's primary east-west corridor, this section of E. Main Street had an average daily traffic count of 13,600 vehicles in 2010. This stretch of E. Main Street is fronted by some residential, commercial, and industrial properties, but it also passes several open, undeveloped parcels. Its curb-to-curb width is 52 feet near

Alonzo & Black Otter Parks, but further east, the street transitions to a rural cross section with shoulders and ditches. Its right-of-way width varies from 85 to 114 feet throughout this area.

Intersections with S. Lake Street and Kelly Way also appear along this stretch of E. Main Street. Although it is not the primary focus for Segment 04, a linkage between Segment 01, the Pine Groves Estates subdivision, and Greendale Road could be considered in the future. This area includes some wetlands and surface water (including manmade ponds), as well as an existing trail system within Pine Groves Estates.

### Design

As is shown in the concept plan below, Segment 04 is a proposed sidepath connecting the east end



Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for Segment 04 and the key features included in its concept plan.

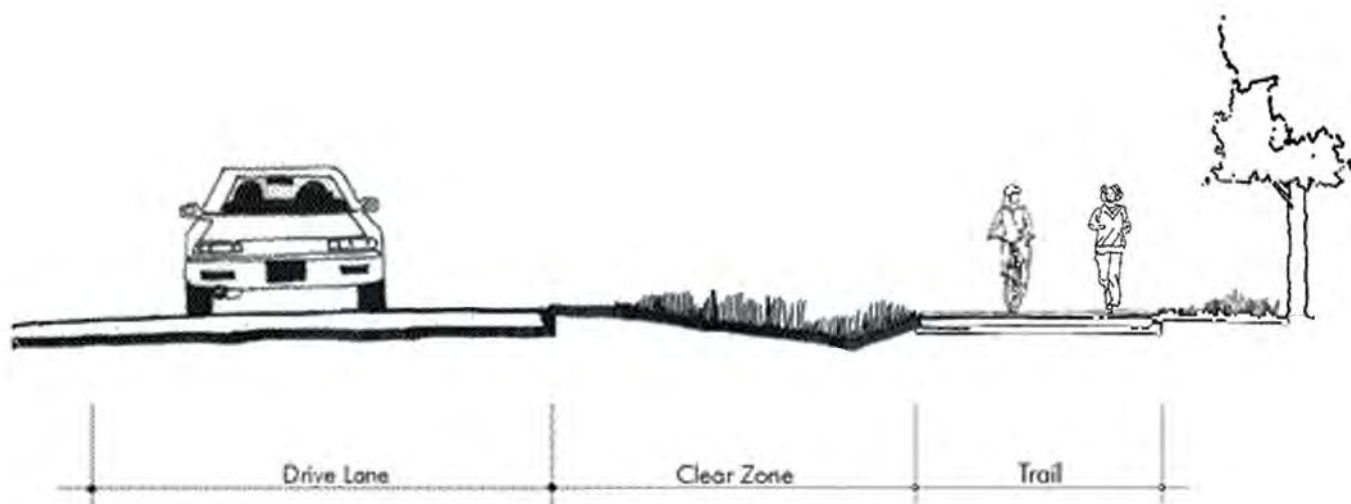


of the Village (Greendale Road) to Alonzo & Black Otter Parks. Sidepaths are multi-use trails that immediately parallel roadways, like extra-wide sidewalks. It would feature a paved surface, made of asphalt or concrete, over its entire length, which would be approximately 3,650 feet (or .69 miles). This segment should align and be consistent with the future 10-foot wide multi-use trail on the south side of WIS 15, which will connect the east end of Hortonville to Julius Drive in Greenville as a part WisDOT's bypass project. As the roadway transfers to a county highway with the WIS 15 bypass project, collaboration with the Outagamie County Highway Department will also be necessary. Adding marked crossings and improving wayfinding signage are other components of the Segment 04 concept. The following are some general guidelines/considerations for Segment 04; however, project engineering will specify exact design requirements (modified from *Iowa Trails 2000*).

- Contrary to popular belief, a sidepath rider is more likely to be involved in a collision with a motor vehicle than a cyclist on the street. Thus, for non-motorized user safety, sidepaths should be considered only when the following criteria can be met:
  - There are relatively few intersecting streets and there are good sight triangles, including driveways.
  - The path parallels a high-speed or high-volume roadway with poor on-street riding

conditions.

- There is 15 to 20 feet of barrier-free space adjacent to the road for path and clear zones.
- Accessible facilities should be developed that meet the needs of older adults and people with disabilities (as possible while maintaining the essential character of the resource).
- Recommended trail width of 10 feet.
- Maintain a minimum 2-foot "clear zone" on each side of the trail.
- Trail should maintain an 8-foot minimum vertical clearance.
- The trail surface should be smooth and free of tread obstacles.
- Placement of snowmobile trails on asphalt surfaces should be avoided, as studs will cause damage to the asphalt.
- Trail should be well drained; standing water on the trail will decrease its life and quality.
- The distance a user can see along the trail should never be less than the distance it would take that user to stop.
- Recommended grade no steeper than 5 percent.
- Signage should be based on the FHWA's *Manual on Uniform Traffic Control Devices*, and be placed outside the recommended clear zones for both trails and roadways.
- All roadway and driveway crossings should include clear sightlines and flush transitions between the trail and crossing. Roadway



*Conceptual cross section of potential sidepath along Segment 04 (modified from Iowa Trails 2000).*

crossings should also include a 3-foot strip of detectable warnings (i.e. truncated domes) as defined in the Americans with Disabilities Act Accessibility Guidelines (ADAAG).

- The following elements should be included in crossings of residential streets – (a) cautionary and regulatory signage on trail, (b) cautionary signage on roadway, and (c) marked crosswalks if trail or roadway traffic volume is high, or if safety concerns exist.
- The following elements should be included in crossings of access drives – (a) cautionary signage on trail and (b) trail may be striped across driveway.

## Access

Unlike some of the other connections featured in this document, no land acquisition or easements will be needed to move forward with Segment 04. In order to carry out the concepts included here, however, some land within the existing public right-of-way would be built upon to accommodate non-motorized users. Although the exact location and alignment of Segment 04 could vary upon implementation, below is a table of the current right-of-way included in the concept plan.

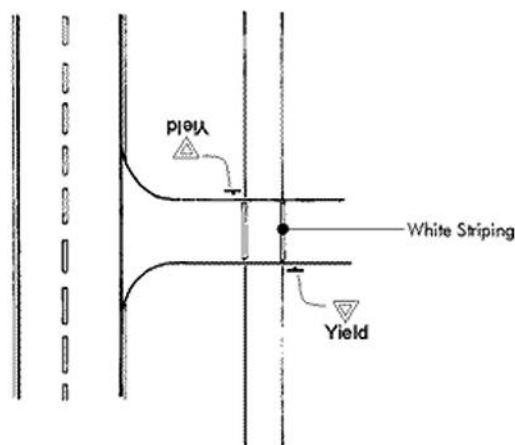
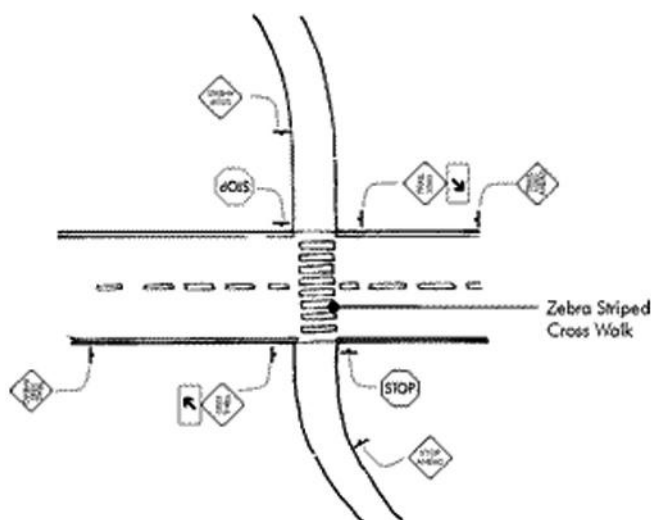
| Street Name             | Approximate Distance | Percent of Total |
|-------------------------|----------------------|------------------|
| WIS 15 / E. Main Street | 3,650 feet           | 100%             |
| <b>TOTAL</b>            | <b>3,650 feet</b>    | <b>100%</b>      |

## Uses

The concept plan for Segment 04 invites walkers, joggers, bicyclists, and in-line skaters to utilize the paved trail facility. This is a common example of a multi-use trail, and conflicts are relatively rare. Regardless, education of the various trail users and promoting safe, responsible behavior will help minimize trail conflicts. For example, trail etiquette standards can be publicized through signage and other materials (i.e. how faster-moving bicyclists and skaters should pass pedestrians).

## Cost

A list of general costs for elements typically included in trail projects is shown on page 32. This may serve as a helpful resource as features included in the Segment 04 concept plan, such as paved sidepath, marked crossings, and wayfinding signage, are evaluated further.



Left: Conceptual crossing layout for collector or residential streets. Above: Conceptual crossing layout for access drives (both images modified from Iowa Trails 2000).

## Segment 05: Miller Park to S. Nash Street (Downtown)

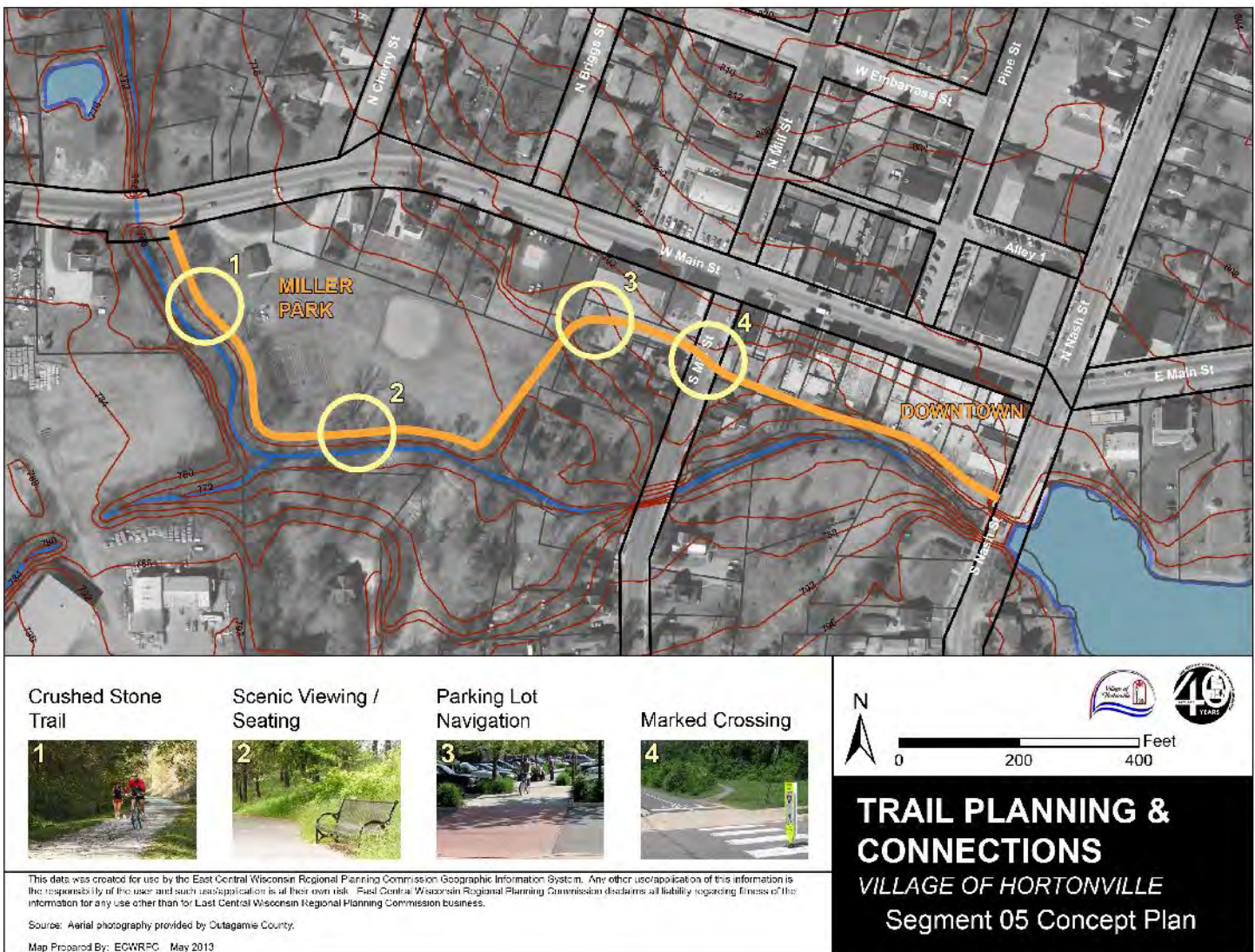
### Site Description

The area included in Segment 05 is located in the central part of the Village, found mostly within Miller Park and along the backside of downtown businesses. Here, Black Otter Creek flows west as it drains from Black Otter Lake, creating some sloping topography. Aside from the two bridges crossings (S. Mill Street and S. Nash Street), relatively thick vegetation is also found along much of the creek's edge. Through downtown, W. Main Street is lined mostly with small offices, retail, and service-oriented businesses, such as restaurants and taverns. Found behind these buildings are

mostly “back of house” functions, including waste disposal and parking.

### Design

As is shown in the concept plan below, Segment 05 features a multi-use trail running behind the businesses on the south side of W. Main Street to form a link between Miller Park and S. Nash Street in downtown. Initially, it would feature a crushed stone surface (aside from street and parking lot crossings), but it could eventually be paved with asphalt or concrete in the future. The full length would be approximately 1,820 feet (or .34 miles). Since the proposed trail travels adjacent to Black Otter Creek, coordination with WDNR and Outagamie County may be necessary. Adding trailside seating and installing signage / pavement



Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for Segment 05 and the key features included in its concept plan.



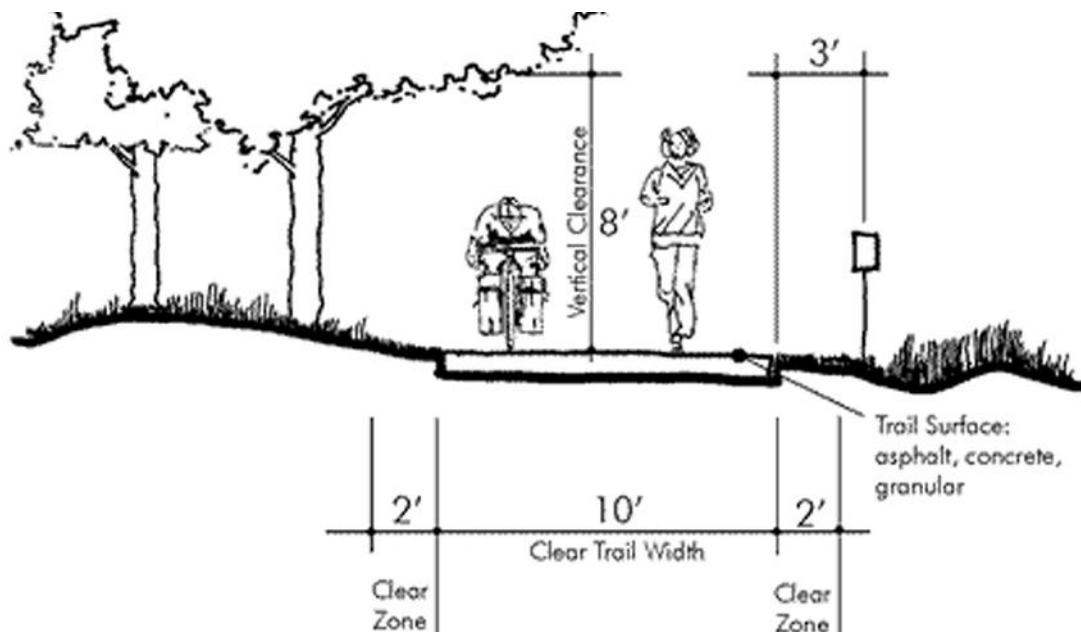
markings to facilitate safe navigation across driveways and parking lots are other components of the Segment 05 concept. The following are some general guidelines/considerations for Segment 05; however, project engineering will specify exact design requirements (modified from *Iowa Trails 2000*).

- Accessible facilities should be developed that meet the needs of older adults and people with disabilities (as possible while maintaining the essential character of the resource).
- Recommended trail width of 10 feet.
- Maintain a minimum 2-foot “clear zone” on each side of the trail.
- Trail should maintain an 8-foot minimum vertical clearance.
- The trail surface should be smooth and free of tread obstacles.
- Trail should be well drained; standing water on the trail will decrease its life and quality.
- The distance a user can see along the trail should never be less than the distance it would take that user to stop.
- Recommended grade no steeper than 5 percent.
- Signage and pavement markings should be based on the FHWA's *Manual on Uniform Traffic Control Devices*, and signage should be placed outside the recommended clear zones for both trails and roadways.

- All roadway and driveway crossings should include clear sightlines and flush transitions between the trail and crossing. Roadway crossings should also include a 3-foot strip of detectable warnings (i.e. truncated domes) as defined in the Americans with Disabilities Act Accessibility Guidelines (ADAAG).
- The following elements should be included in crossings of residential streets – (a) cautionary and regulatory signage on trail, (b) cautionary signage on roadway, and (c) marked crosswalks if trail or roadway traffic volume is high, or if safety concerns exist.
- The following elements should be included in crossings of access drives – (a) cautionary signage on trail and (b) trail may be striped across driveway.
- Possible options for parking lot navigation:
  - Bike Route signs should be placed in advance of all turns or decision points. They should include destinations, directional arrows, and distance.
  - The Shared Lane Marking in use within the U.S. is the bike-and-chevron “sharrow.”
- Depending on how far the trail is set back from the creek, some fencing (42-inch with a side barrier) may be necessary.

### Access

In order to carry out the concepts included here, access to land – with various owners – will need to be granted. This could be accomplished in several



*Conceptual cross section of potential multi-use, crushed stone trail along Segment 05 (modified from Iowa Trails 2000).*

ways, including direct acquisition (purchase) and/or establishing easements. Although the exact location and alignment of Segment 05 could vary upon implementation, below is a table of current property owners included in the concept plan.

| Property Owner           | Approximate Distance | Percent of Total |
|--------------------------|----------------------|------------------|
| Village of Hortonville   | 990 feet             | 54%              |
| Dufrane Properties, LLC  | 55 feet              | 3%               |
| Village of Hortonville   | 200 feet             | 11%              |
| S. Mill Street           | 55 feet              | 3%               |
| Village of Hortonville   | 130 feet             | 7%               |
| Robert Sell              | 15 feet              | 1%               |
| Village of Hortonville   | 140 feet             | 8%               |
| Marsha Griesbach         | 30 feet              | 2%               |
| Timothy & Monica Guyette | 85 feet              | 5%               |
| Village of Hortonville   | 120 feet             | 6%               |
| <b>TOTAL</b>             | <b>1,820 feet</b>    | <b>100%</b>      |

## Uses

The concept plan for Segment 05 invites walkers, joggers, bicyclists, and in-line skaters (if paved) to utilize the trail facility. This is a common example of a multi-use trail, and conflicts between these user groups are relatively rare. However, trail users will also be expected to interact with motorists where portions of the trail traverse parking lots and access drives. Here, signage, pavement markings, and other educational materials can be used to promote safe, responsible behavior.

## Cost

A list of general costs for elements typically included in trail projects is shown on page 32. This may serve as a helpful resource as features included in the Segment 05 concept plan, such as crushed stone trail, benches, and marked crossings, are evaluated further.



*Conceptual sketch of potential bike parking at the ends of Segment 05 (modified from Iowa Trails 2000).*

## Segment 06: Veterans Memorial Park to Fishing Pier (Downtown)

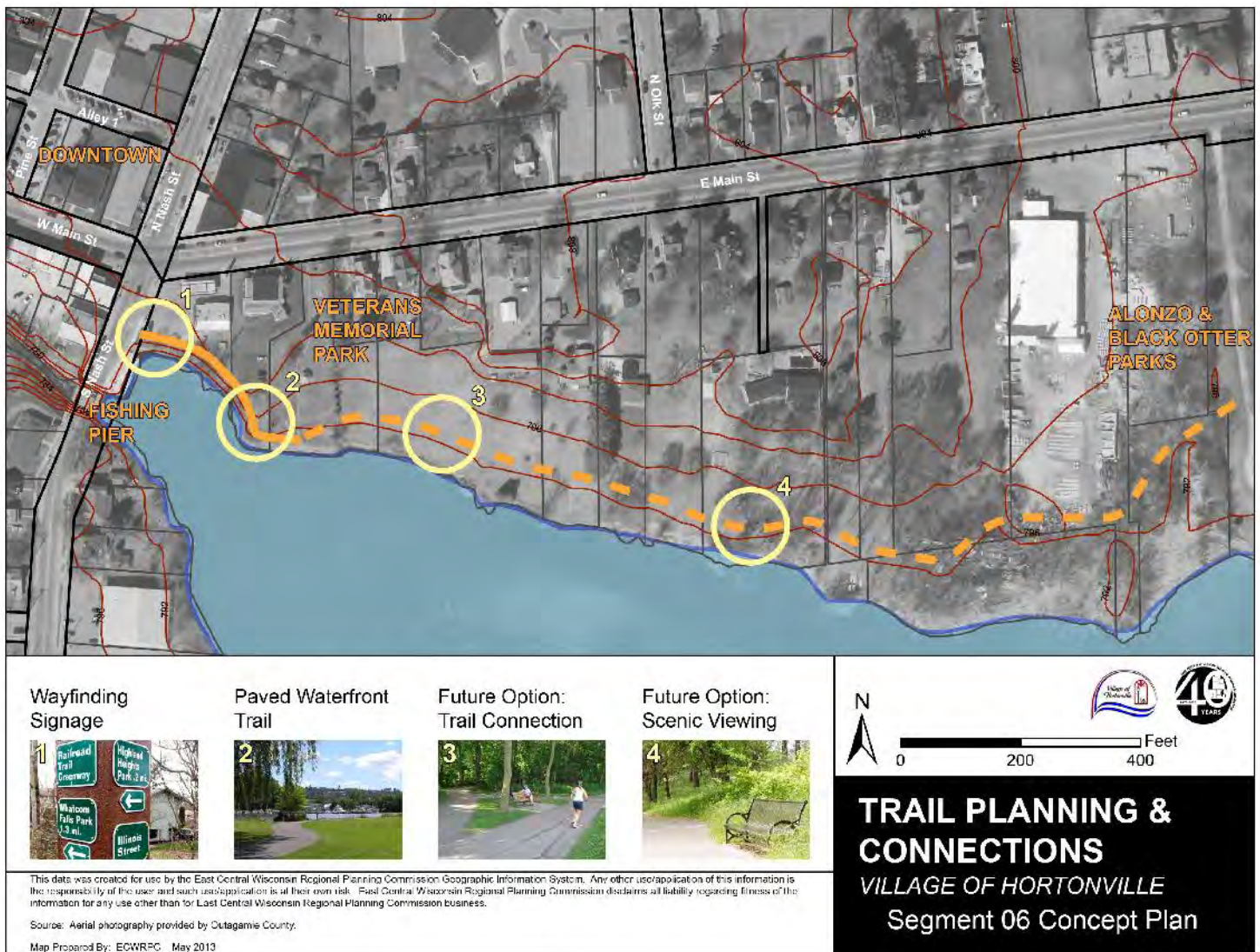
### Site Description

The area included in Segment 06 is located in downtown Hortonville, on the northern shore of Black Otter Lake. Immediately west is the S. Nash Street bridge and dam between the lake and creek. Currently, commercial properties facing E. Main Street separate two of the community's recreational draws – Veterans Memorial Park and the fishing pier. Although it is not the primary focus for Segment 06, a linkage between Veterans Memorial Park and Alonzo & Black Otter Parks could also be considered in the future. This area includes a variety of residential and industrial properties fronting E. Main Street, which leaves a

considerable amount of private open space along the water.

### Design

As is shown in the concept plan below, Segment 06 focuses on a shoreline, multi-use trail connection between Veterans Memorial Park and the fishing pier downtown. It would feature a paved surface, made of asphalt or concrete, over its entire length, which would be approximately 350 feet (or .07 miles). Since the proposed trail travels adjacent to Black Otter Lake, coordination with WDNR and Outagamie County may be necessary. Enhancing wayfinding signage and adding trailside seating are other possible components of the Segment 06 concept. The following are some general guidelines/considerations for Segment 06;



Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for Segment 06 and the key features included in its concept plan.

however, project engineering will specify exact design requirements (modified from *Iowa Trails 2000*).

- Accessible facilities should be developed that meet the needs of older adults and people with disabilities (as possible while maintaining the essential character of the resource).
- Recommended trail width of 10 feet.
- Maintain a minimum 2-foot “clear zone” on each side of the trail.
- Trail should maintain an 8-foot minimum vertical clearance.
- The trail surface should be smooth and free of tread obstacles.
- At each end of the trail, there should be a flush transition with the existing sidewalk and park path.
- Placement of snowmobile trails on asphalt surfaces should be avoided, as studs will cause damage to the asphalt.
- Trail should be well drained; standing water on the trail will decrease its life and quality.
- The distance a user can see along the trail should never be less than the distance it would take that user to stop.
- Recommended grade no steeper than 5 percent.
- Signage should be based on the FHWA's *Manual on Uniform Traffic Control Devices*, and be placed outside the recommended clear zones for both trails and roadways.
- Depending on how far the trail is set back from the lake, some fencing (42-inch with a side barrier) may be necessary.
- Rip-rap may be needed along the lake’s edge for shoreline protection.

Access

In order to carry out the concepts included here, access to land – with various owners – will need to

be granted. This could be accomplished in several ways, including direct acquisition (purchase) and/or establishing easements. Although the exact location and alignment of Segment 06 could vary upon implementation, below is a table of current property owners included in the concept plan.

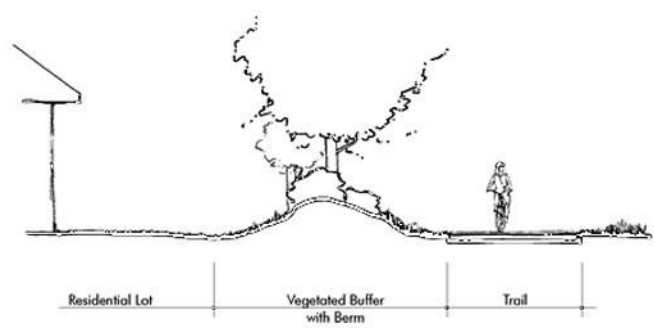
| Property Owner            | Approximate Distance | Percent of Total |
|---------------------------|----------------------|------------------|
| A & N Hortonville BP, LLC | 175 feet             | 50%              |
| BMO Harris Bank           | 125 feet             | 36%              |
| BMO Harris Bank           | 50 feet              | 14%              |
| <b>TOTAL</b>              | <b>350 feet</b>      | <b>100%</b>      |

Uses

The concept plan for Segment 06 invites walkers, joggers, bicyclists, and in-line skaters to utilize the paved trail facility. This is a common example of a multi-use trail, and conflicts are relatively rare. Regardless, trail etiquette standards could be publicized through signage and other materials to reinforce safe, responsible behavior by all trail users.

Cost

A list of general costs for elements typically included in trail projects is shown on page 32. This may serve as a helpful resource as features included in the Segment 06 concept plan, such as paved trail and wayfinding signage, are evaluated further.

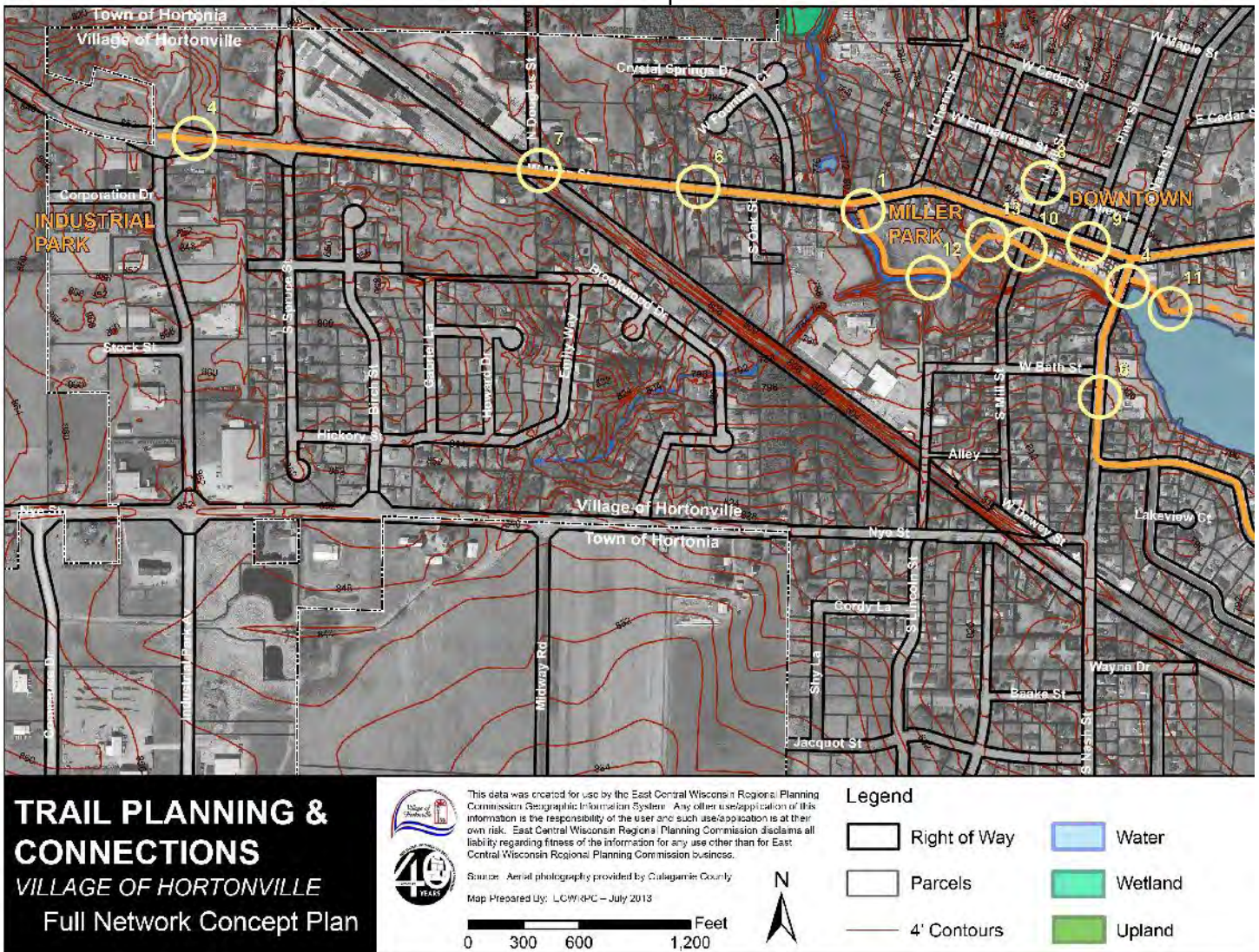
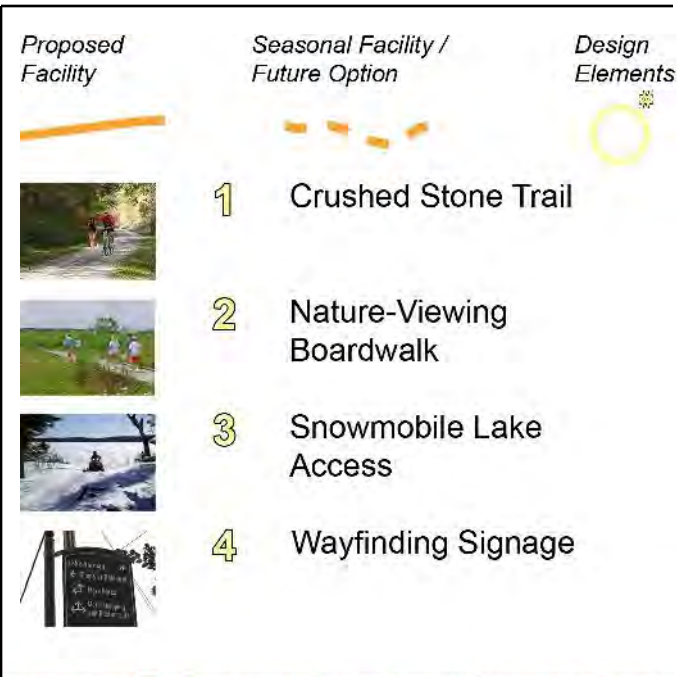


Above: Sample wayfinding signage design. Right: Conceptual cross section of trailside buffer along Segment 06 (both images modified from *Iowa Trails 2000*).



## The Full Network

Although the six segments are described individually and in greater detail on previous pages, it is also important to consider how each concept plays an important role in advancing the goal of a more complete multi-modal network within the Village of Hortonville. As is shown in the concept plan below, the segments of trails and other facilities work collectively to create better connections for non-motorized users traveling to/from major community origins and destinations. Individually, each segment could be seen as one “phase” of expanding the full network. As they are developed, thoughtful consideration should be given to the transition between segments (i.e. from trail to bike lane).



Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for the full network and the key features included in its concept plan.





5 Sharrows / Route



6 Bike Lanes



7 Rail Crossing



8 Bike Parking



9 Curb Extensions



10 Marked Crossing



11 Paved Trail



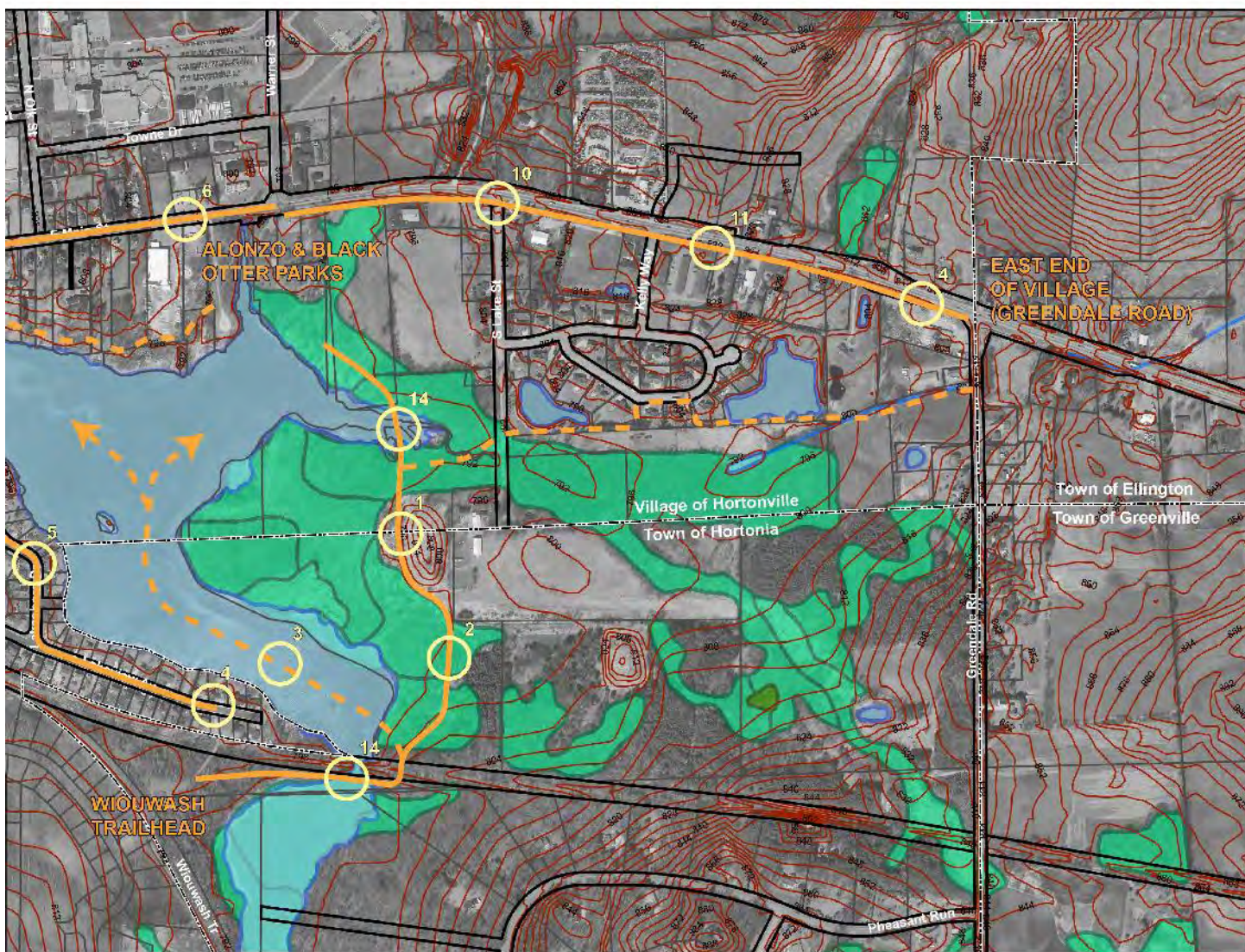
12 Scenic Viewing / Seating



13 Parking Lot Navigation



14 Multi-Use Trail Bridge



Continued — Map displaying the existing conditions (right of way, parcels, topography, water, wetlands, etc.) for the full network and the key features included in its concept plan.



## General Cost Considerations

One of the major considerations during the design and implementation of trail and on-street accommodations is cost. The potential cost of a

facility may have implications on what types of funding are used, whether the project is phased or built all at once, and what surface is installed. It is important to have a general understanding of these costs at the inception of the project in order to seek

| ELEMENT   | UNIT      | PRICE PER UNIT<br>(year 2013 Dollars) |
|---|-----------|---------------------------------------|
| <b>Trail Construction</b>                         |           |                                       |
| Clearing and grubbing                             | Acre      | \$2,710.00                            |
| Excavation  | Cu. Yd.   | \$19.50                               |
| Grading for hard-surfaced trails                  | Mile      | \$4,060.00                            |
| Granular subbase (4" depth)                       | Sq. Ft.   | \$0.55                                |
| Crushed stone surfacing (4" depth)                | Sq. Ft.   | \$0.55                                |
| Asphalt surfacing (4" depth)                      | Sq. Ft.   | \$1.35                                |
| Concrete surfacing (4" depth)                     | Sq. Ft.   | \$3.05                                |
| Boardwalk   | Sq. Ft.   | \$67.65                               |
| Bridge deck (concrete or steel)                   | Sq. Ft.   | \$117.85                              |
| Bridge abutments                                  | Each      | \$22,370.00                           |
| Seeding/mulching                                  | Acre      | \$2,165.00                            |
| Other costs (drainage, signage, support services) |           | 10% of trail cost                     |
| <b>On-Street Facility Construction</b>            |           |                                       |
| Lane striping                                     | Mile      | \$4,230.00                            |
| Bicycle arrow                                     | Each      | \$68.65                               |
| Bicycle symbol                                    | Each      | \$91.95                               |
| Shared lane marking (sharrow)                     | Each      | \$91.95                               |
| Sign with post                                    | Each      | \$260.00                              |
| Curb extension                                    | Each      | \$15,000.00                           |
| Marked crosswalk (ladder style)                   | Each      | \$325.00                              |
| Curb ramp   | Each      | \$1,385.00                            |
| <b>Misc. Equipment</b>                            |           |                                       |
| Bicycle rack (inverted U -- 2 bicycles)           | Each      | \$250.00                              |
| Bicycle rack (ribbon -- 6 bicycles)               | Each      | \$505.00                              |
| Bench   | Each      | \$750.00                              |
| <b>Real Estate Acquisition</b>                    |           |                                       |
| Rural/undeveloped                                 | Acre      | \$2,945.00                            |
| Suburban/single-family residential                | Acre      | \$51,490.00                           |
| Urban/high-density residential                    | Sq. Ft.   | \$16.85                               |
| Urban/central business district                   | Sq. Ft.   | \$44.05                               |
| <b>Design &amp; Administration</b>                |           |                                       |
| Planning  |           | 2% of trail cost                      |
| Design/engineering                                |           | 10% of trail cost                     |
| Field Inspection                                  |           | 2% of trail cost                      |
| Administration                                    |           | 5% of trail cost                      |
| <b>Operations &amp; Maintenance</b>               |           |                                       |
| Maintenance                                       | Mile/year | \$8,425.00                            |

Table of rough unit cost estimates for elements often included in trail and/or on-street facility projects (modified from Iowa Trails 2000 and Albemarle, NC Pedestrian Plan (2007)).

adequate funding and budget accordingly. The table below provides an overview of rough cost estimates for elements often included in trail and/or on-street facility projects. However, these construction costs can vary due to a variety of factors, including local conditions, trail type, and other support services incorporated in the project. Therefore, this table is simply a general guideline for the purpose of preliminary estimation of costs. More detailed cost estimation should be performed at other points in the project implementation process, particularly at the time of application for funding, during preliminary design, and prior to bidding for construction (modified from *Iowa Trails 2000*).

## Other Considerations

The placement, design, and cost of a project will be unique to each individual segment, as outlined in the previous pages. However, the four overarching items covered below apply generally to the overall network and will be beneficial to consider as things move forward.

### Construction Techniques

Project construction or installation can be accomplished in a variety of ways, including: (1) utilizing Village equipment and personnel, (2) hiring a professional contractor, (3) bringing together a group of trail construction volunteers, or (4) integrating all of the above options. Regardless, a trail surface can be created with minimal environmental disturbance if the following are met (modified from *Northern Utah Bonneville Shoreline Trail Master Plan (2002)* – <http://bonnevilleshorelinetrail.org/resources/bst%20north%20master%20plan/Northern%20Utah%20BST%20Master%20Plan.htm>):

- Minimize soil disturbance and eliminate the potential for erosion.
- Use arboriculturally correct pruning or removal of tree limbs and shrubs.
- Maintain existing drainage patterns; do not force nature.
- Coordinate excavation with vegetation and drainage considerations.
- Attain proper slope and compaction through a detailed analysis of on-site conditions during wet and dry periods.
- Retain dead standing trees (commonly known as snags) when safety permits because wildlife use trails and snags offer homes and feeding locations for many bird and mammal species.

Consider erecting nest boxes or creating artificial snags in woodlands near the trail route.

### Signage & Wayfinding

The inclusion of signage along trails and on-street facilities is an important amenity not to be overlooked, as it increases user safety and comfort. Signs serve a variety of purposes, such as assisting in the navigation of a trail or trail system, warning of approaching crossings, regulating trail use, or interpreting natural features. The Federal Highway Administration's *Manual on Uniform Traffic Control Devices (2009)* is an excellent reference for standard signage. The following are general guidelines related to signage (modified from *Iowa Trails 2000* and WisDOT's *Wisconsin Bicycle Facility Design Handbook*).

- Signs should be placed where they will be clearly visible. Placement is dependent on the sight lines (relative to user speed) of each trail.
- Signs should be placed at a constant distance from the trail edge.
- Lettering less than two inches in height is not recommended for directional signs.
- Multiple signs may be mounted on the same post, but the primary message should be in the top position on the post.
- Permanent signage may be supplemented with removable seasonal snowmobile signage, if necessary.
- Bike route signs may be used on streets with bike lanes, as well as on multi-use trails. This is especially important for wayfinding purposes if a bikeway transitions from one type to another throughout a community. For example, if a particular segment of a community's bikeway consists of a multi-use trail, then continues to a set of bike lanes, then finishes as a shared roadway, it may be advantageous to use bike route signs to tie in all three bikeway types together and aid users in finding their way. Signed routes may be partnered with a printed and/or online route map.
- Wayfinding signage should identify the direction, distance, and perhaps estimated time, required to travel to community destinations found along the network.

### Maintenance

A maintenance program helps preserve the quality of a trail or on-street facility and ensure the safety

of its users. Programs such as “Adopt a Trail” encourage local volunteers to maintain a section of trail as a community service. Prioritization of trail maintenance tasks are: (1) to correct unsafe trail conditions, (2) to repair environmental damage, and (3) to restore the trail to the desired conditions. Mowing and trash removal are examples of typical ongoing maintenance activities. Other annual tasks often include the following (modified from *Northern Utah Bonneville Shoreline Trail Master Plan*):

- Clear windfalls from the trail bed.
- Remove loose rocks and debris from the tread surface.
- Repair trail wash-outs.
- Remove new plant growth on the trail.
- Level the trail tread as necessary and restore the tread grade to the original slopes. Fill ruts, holes, low spots, or muddy areas.
- Check for erosion effects after spring runoff and repair erosion-damaged facilities as needed.
- Check and repair all structures after spring runoff and after severe storms.
- Check, repair, or replace signs and trail markers prior to the major use season.
- If deemed appropriate, plow snow from hard-surfaced trails and on-street facilities on a seasonal basis.
- Where snowmobiling and/or cross-country skiing is allowed, groom trails to allow for adequate use. In many cases, volunteers or existing clubs can groom trails.

### Support Services

Besides the trail and/or on-street facility, there are other accommodations that can increase the quality of the user experience. These amenities are collectively known as support services, and they fall into three general types (modified from *Iowa Trails 2000*):

- Trailheads and access points provide a means of accessing a trail. Trailheads are often located at the terminus of a trail corridor, as already exists for the Wiouwash Trail, and access points are typically placed adjacent to schools, commercial areas, and parks.
- Rest areas are places to stop and rest off the main traveled way. They may include a seating area, drinking water, restrooms, and signage. In Hortonville, many of the “origins” and

“destinations” may already function as rest areas.

- Interpretive facilities provide an opportunity to educate users on various aspects of the landscape, including native plants and animals, geologic history, and local history. They often include signage wherever there is a significant cultural, historical, or natural feature.



*Photo of interpretive signage along the Richland Creek Greenway in Waynesville, NC.*



## NEXT STEPS

### *Funding Opportunities*

Additional funding will likely be required in order to implement the trail and on-street facility concepts included in this report. There are numerous existing grant programs and a variety of innovative funding techniques that provide financing for projects such as these. However, these funding opportunities are increasingly competitive, and the money requested is often much greater than the amount available. Acquiring funding for projects is more likely if it can be leveraged with a variety of local, state, federal and public/private sources. Listed below, and displayed in a table on the following page, are some potential funding sources available for trail and on-street facility projects (modified from *Shawano County Bicycle and Pedestrian Master Plan*). The Village of Hortonville should track grant and other funding opportunities and utilize this report to respond to announcements.

#### Federal Funds

The largest source of federal funding for bicycle and pedestrian projects is the United States Department of Transportation's (US DOT) Federal-Aid Highway Program. The latest act, Moving Ahead for Progress in the Twenty-First Century (MAP-21) was enacted in July 2012. MAP-21 authorizes funding for federal surface transportation programs including highways and transit until September 2014. There are a number of programs identified within MAP-21 that are applicable to bicycle and pedestrian projects, including:

- Transportation Alternatives Program (TAP)
  - Transportation Enhancements
  - Bicycle & Pedestrian Facilities Program
  - Recreational Trails Program
  - Safe Routes to School
- Surface Transportation Program (STP)
- Highway Safety Improvement Program (HSIP)
- Congestion Mitigation/Air Quality Program (CMAQ)

Other related federal grant programs are listed below. Further information and additional opportunities are found on the "MAP-21 Find It, Fund It!" chart on page 40.

- Partnership for Sustainable Communities
- Community Development Block Grants
- Community Transformation Grants
- Land and Water Conservation Fund
- Rivers, Trails, and Conservation Assistance Program

#### State of Wisconsin Funds

In the past, the State of Wisconsin has funded bicycle and pedestrian projects above and beyond Federal Transportation Enhancement (TE) dollars through WisDOT's Bicycle and Pedestrian Facilities Program (BPFP). Funding through the program is competitive, and all BPFP funds have been awarded through FY 2014. However, the TE program and BPFP were impacted by MAP-21. With certain exceptions, TE and BPFP projects generally remain eligible for funding within the Transportation Alternatives Program (TAP), a more comprehensive funding category. WisDOT tentatively plans to begin the next TAP awards cycle by October 2013.

WDNR administers several grant programs that may support bicycle and pedestrian facilities that provide a recreational benefit to the state. These programs include:

- Recreational Trails Aid (RTA) Program
- Knowles-Nelson Stewardship Program
  - State Trails
  - Acquisition and Development of Local Parks
  - Friends of State Lands
  - Habitat Areas
  - Urban Green Space
  - Urban Rivers

#### Other Potential Funding Sources

Private foundations, such as the Bikes Belong Coalition, are an increasingly important source of funds for bicycle and pedestrian planning and implementation. Soliciting corporate sponsorship, by allowing businesses to put their name/logo on trail signage in exchange for donations, is another innovative approach to project funding. Partnering with local non-profit groups, like Fox Cities

Greenways, to coordinate fundraising efforts could also help pull together money for specific projects.

## Key Partnerships

The vision for trails and on-street facilities, as set forth in this report, will be implemented through a cooperative effort between state agencies, regional agencies, local governments, trails advocacy groups, and non-profit organizations. Continued coordination of efforts is necessary in order to maximize funding, efficiently use staff time, and market completed projects. Found below is an

initial list of potential partners; their input and support will likely be needed to advance from idea to implementation. In addition to the stakeholders listed, impacted property owners and other members of the public should be further involved as things move forward.

Village of Hortonville:

- Traci Martens, Village President
- Al Habeck, Trustee
- Vacant, Village Administrator
- Carl McCrary, Director of Public Works

| Funding Program                                    | On-Street Pedestrian Facilities | On-Street Bicycle Facilities | Off-Street Multi-Use Trail |
|--|---------------------------------|------------------------------|----------------------------|
| Transportation Alternatives Program (TAP)          | X                               | X                            | X                          |
| Surface Transportation Program (STP)               | X                               | X                            | X                          |
| Highway Safety Improvement Program (HSIP)          | X                               | X                            | X                          |
| Congestion Mitigation / Air Quality (CMAQ)         | X                               | X                            | X                          |
| Partnership for Sustainable Communities            | X                               | X                            | X                          |
| Community Development Block Grants (CDBG)          | X                               |                              |                            |
| Community Transformation Grants (CTG)              | X                               | X                            | X                          |
| Land and Water Conservation Fund (LWCF)            |                                 |                              | X                          |
| Rivers, Trails, and Conservation Assistance (RTCA) |                                 |                              | X                          |
| Recreational Trails Aid Program (RTA)              |                                 |                              | X                          |
| Knowles-Nelson Stewardship Program:                |                                 |                              |                            |
| State Trails                                       |                                 |                              | X                          |
| Acquisition and Development of Local Parks         |                                 |                              | X                          |
| Friends of State Lands                             |                                 |                              | X                          |
| Habitat Areas                                      |                                 |                              | X                          |
| Urban Green Space                                  |                                 |                              | X                          |
| Urban Rivers                                       |                                 |                              | X                          |
| Private Foundations                                | X                               | X                            | X                          |
| Corporate Sponsorship                              | X                               | X                            | X                          |
| Non-Profit Fundraising                             | X                               | X                            | X                          |

*Table summarizing the type of projects various funding sources can cover (modified from Shawano County Bicycle and Pedestrian Master Plan).*

Wisconsin Department of Transportation:

- Matt Halada, Transportation Planner
- Kim Rudat, Communications Manager for WIS 15 Improvement Project

Wisconsin Department of Natural Resources:

- Christine Halbur, Government Outreach Team Leader
- Gary Hanson, Community Services Specialist

East Central Wisconsin Regional Planning Commission:

- Eric Fowle, Executive Director
- Walt Raith, Assistant Director
- David Kress, Planner

Outagamie County:

- Mike Hendrick, Planning Director
- Brad Bastian, GIS Director
- Christopher Brandt, Parks Director

City of New London:

- Kent Hager, City Administrator
- Jeff Bodoh, Director of Public Works
- Chad Hoerth, Director of Parks & Recreation

Town of Greenville:

- Dave Tebo, Town Administrator
- Dean Schiller, Public Works Supervisor
- Tony Nowak, Director of Parks and Forestry

Town of Hortonia:

- Bob Luedtke, Chairman

Black Otter Lake District:

- Matt Schroeder, Chairperson

Fox Cities Greenways:

- Michael Kading, President

Canadian National Railroad:

- Jackie Macewicz, Manager Public Works

## Potential Timeline

This report provides an initial set of short-to-mid-term concepts that Village decision-makers, as well as other project partners, can use to guide Hortonville's progress towards offering non-motorized users safer and more comfortable travel options within the community. The table below lists the six segments and proposes a rough implementation timeframe for each. However, the order in which projects in this report are built will depend on many factors, including budget and grant availability, community support, and various policies. The Village should pursue implementation of these concepts opportunistically. For example, timing of the facilities along Main Street should coincide with the jurisdictional transfer and work

| Project  | 2013 | 2014 | 2015 | 2016 | 2017 | 2018 |
|--|------|------|------|------|------|------|
| Segment 01:<br>Wiouwash Trail to Alonzo & Black Otter Parks      |      |      |      |      |      |      |
| Segment 02:<br>Wiouwash Trail to Downtown                        |      |      |      |      |      |      |
| Segment 03:<br>Alonzo & Black Otter Parks to Industrial Park     |      |      |      |      |      |      |
| Segment 04:<br>East End of Village to Alonzo & Black Otter Parks |      |      |      |      |      |      |
| Segment 05:<br>Miller Park to S. Nash Street                     |      |      |      |      |      |      |
| Segment 06:<br>Veterans Memorial Park to Fishing Pier            |      |      |      |      |      |      |



— Potential continued planning, design, and construction

*Table of rough, proposed timeframe for implementing the various concepts included in this report.*

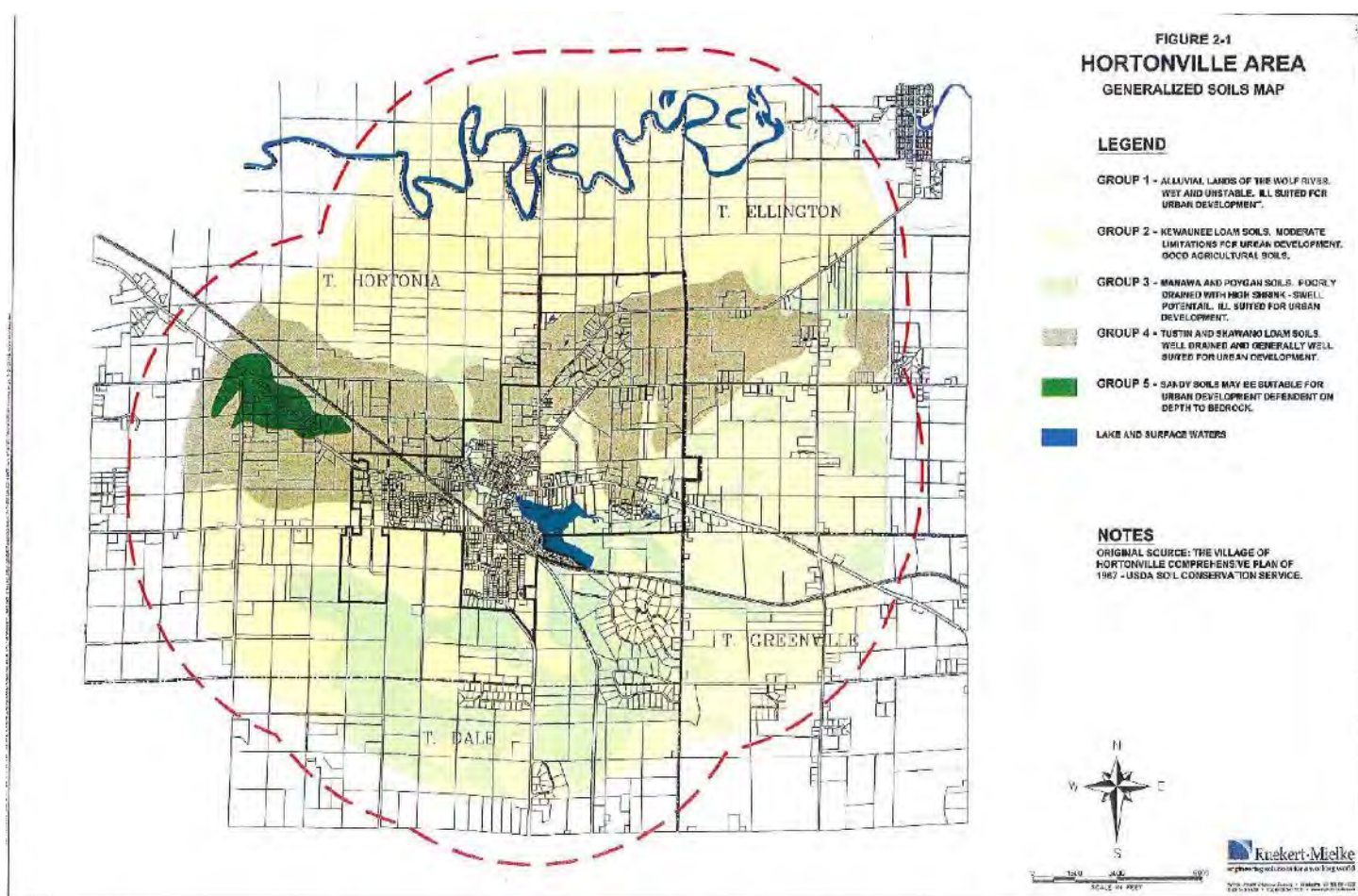


done for the upcoming WIS 15 Improvement Project.

In the coming years, the Village should revisit and assess its progress toward project implementation and include additional concepts, as appropriate. Other connections to neighboring communities, such as New London, would likely be one idea pursued in the future.



# APPENDICES



Generalized Soils Map included in the Village of Hortonville Comprehensive Plan (2003).



## Find It Fund It Table

This table indicates potential eligibility for pedestrian and bicycle projects under federal highway and transit programs. In each case there are specific requirements that must be met within eligibility criteria and eligibility will be determined on a case-by-case basis.

|  | TAP | CMAQ | STP | HSIP | RTP | NHPP | FLTP | TTP | PLA | UZA | 402 | SGR | BBF | 5310 | 5311 | SRTS | BYW | FLH | TCSP | JOBS | FTA | FTA-TE |
|--|-----|------|-----|------|-----|------|------|-----|-----|-----|-----|-----|-----|------|------|------|-----|-----|------|------|-----|--------|
| Bicycle and Pedestrian plan  |     |      | X   |      |     |      |      |     | X   |     |     |     |     |      | X    |      |     |     | X    |      | X   |        |
| Bicycle lanes on roadway   | X   | X    | X   | X    |     | X    | X    | X   |     | X   |     |     |     |      |      | X    | X   | X   | X    |      | X   | X      |
| Paved shoulders  | X   | X    | X   | X    |     | X    | X    | X   |     | X   |     |     |     |      |      | X    | X   | X   | X    |      |     |        |
| Signed bike route  | X   | X    | X   |      |     | X    | X    | X   |     | X   |     |     |     |      |      | X    | X   | X   | X    |      | X   |        |
| Shared use path/trail  | X   | X    | X   | X    | X   | X    | X    | X   |     |     |     |     |     |      |      | X    | X   | X   | X    |      | X   |        |
| Single track hike/bike trail   | X   |      | X   |      | X   |      |      |     |     |     |     |     |     |      |      |      |     | X   | X    |      |     |        |
| Spot improvement program   | X   | X    | X   | X    |     |      |      |     |     |     |     |     |     |      |      | X    |     |     | X    |      | X   |        |
| Maps   | X   | X    | X   |      |     |      |      |     |     |     | X   |     |     |      |      | X    |     |     | X    |      | X   |        |
| Bike racks on buses  | X   | X    | X   |      |     |      | X    | X   |     | X   |     | X   | X   |      | X    |      |     |     | X    |      | X   | X      |
| Bicycle parking facilities   | X   | X    | X   |      |     |      | X    | X   |     | X   |     | X   | X   |      | X    | X    | X   |     | X    |      | X   | X      |
| Bicycle share<br>(capital and equipment costs only; operations not eligible) | X   | X    | X   |      |     | X    | X    | X   |     |     |     |     |     |      |      |      |     | X   | X    | X    | X   | X      |
| Bicycle storage/service center   | X   | X    | X   |      |     |      | X    | X   |     | X   |     | X   | X   |      | X    | X    |     |     | X    | X    | X   | X      |
| Sidewalks, new or retrofit   | X   | X    | X   | X    |     | X    | X    | X   |     | X   |     |     |     | X    |      | X    | X   | X   | X    |      | X   | X      |
| Crosswalk, new or retrofit   | X   | X    | X   | X    |     | X    | X    | X   |     | X   |     |     |     | X    |      | X    | X   | X   | X    |      | X   | X      |
| Trail/highway intersection   | X   | X    | X   | X    | X   | X    | X    | X   |     |     |     |     |     |      |      | X    | X   | X   | X    |      |     |        |
| Signal improvements  | X   | X    | X   | X    |     | X    | X    | X   |     |     |     |     |     |      |      | X    |     |     | X    |      | X   | X      |
| Curb cuts and ramps  | X   | X    | X   | X    |     | X    | X    | X   |     | X   |     |     |     | X    |      | X    |     |     | X    |      | X   | X      |
| Traffic calming  | X   |      | X   | X    |     |      | X    | X   |     |     |     |     |     |      |      | X    |     |     | X    |      | X   | X      |
| Coordinator position   |     | X    |     |      |     |      |      |     |     |     |     |     |     |      |      | X    |     |     |      |      |     |        |
| Safety/education position  | X   |      | X   |      |     |      |      |     |     |     | X   |     |     |      |      | X    |     |     |      |      |     |        |
| Police patrol  | X   |      |     |      |     |      |      |     |     |     | X   |     |     |      |      | X    |     |     |      |      |     |        |
| Helmet promotion   |     |      | X   |      |     |      |      |     |     |     | X   |     |     |      |      | X    |     |     |      |      |     |        |
| Safety brochure/book   |     |      | X   | X    | X   |      |      |     |     |     | X   |     |     |      |      | X    |     |     |      |      | X   |        |
| Training   |     |      | X   | X    | X   |      |      |     |     |     | X   |     |     |      | X    | X    |     |     |      |      | X   |        |
| Technical Assistance   | X   | X    | X   |      |     |      |      |     |     |     |     |     |     |      | X    | X    |     |     |      |      | X   |        |

## KEY

| Color Code | Meaning  |
|------------|--|
|            | Continuing program & eligibility   |
|            | Funds available until expended, not a continuing distinct program under MAP-21 |

| Abbreviation | Program  |
|--------------|--|
| TAP          | Transportation Alternatives  |
| CMAQ         | Congestion Mitigation and Air Quality Improvement  |
| STP          | Surface Transportation Program   |
| HSIP         | Highway Safety Improvement Program   |
| RTP          | Recreational Trails Program (State may opt out)  |
| NHPP         | National Highway Performance Program   |
| FLTP         | Federal Lands Transportation Program   |
| TTP          | Tribal Transportation Program  |
| PLA          | State/Metropolitan Planning Funds  |
| UZA          | Urbanized Area Formula Program   |
| 402          | State and Community Highway Safety Grant Program   |
| SGR          | State of Good Repair Grant Program   |
| BBF          | Bus and Bus Facilities   |
| 5310         | Enhanced Mobility of Seniors and Individuals with Disabilities   |
| 5311         | Formula Grants for Rural Areas, Rural Transit Assistance Program, and Public Transportation on Indian Reservations |
| SRTS         | Safe Routes to Schools   |
| BYW          | Scenic Byways  |
| FLH          | Federal Lands Highway Program  |
| TCSP         | Transportation, Community and System Preservation  |
| JOBS         | Access to Jobs/Reverse Commute   |
| FTA          | Federal Transit Capital  |
| FTA-TE       | Transit Enhancements   |

“MAP-21 Find It, Fund It!” chart created by Advocacy Advance.

## References

Advocacy Advance “MAP-21 Find It, Fund It!” website

<http://www.advocacyadvance.org/MAP21/finditfundit>

Black Otter Lake District *Adaptive Lake Management Plan* (2008)

City of Albemarle, NC *Comprehensive Pedestrian Plan* (2007)

Federal Highway Administration *Manual on Uniform Traffic Control Devices* (2009)

Iowa Department of Transportation *Iowa Trails 2000: Connecting People and Trails Handbook*  
[http://www.iowadot.gov/iowabikes/trails/web-pdf/Bike-Ped/connecting\\_people\\_trails\\_handbook.pdf](http://www.iowadot.gov/iowabikes/trails/web-pdf/Bike-Ped/connecting_people_trails_handbook.pdf)

*Iowa Trails 2000*  
<http://www.iowadot.gov/iowabikes/trails/iowatrails2000.html>

National Association of City Transportation Officials  
*Urban Bikeway Design Guide*  
<http://nacto.org/cities-for-cycling/design-guide/>

Northern Utah Bonneville Shoreline Trail Master Plan (2002)  
<http://bonnevilleshorelinetrail.org/resources/bst%20north%20master%20plan/Northern%20Utah%20BST%20Master%20Plan.htm>

Seattle Department of Transportation website  
<http://www.seattle.gov/transportation/bikefacilities.htm>

Shawano County *Bicycle and Pedestrian Master Plan* (2013)

*Village of Hortonville Comprehensive Plan* (2003)

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<http://www.hortonvillewi.org/>

Wisconsin Department of Natural Resources  
*Wauwasha State Recreation Trail Master Plan* (1995)  
<http://dnr.wi.gov/topic/lands/masterplanning/documents/MP-PR-WiowashST-1995.pdf>

Wisconsin Department of Natural Resources  
*Wauwasha State Trail* website  
<http://dnr.wi.gov/topic/parks/name/wiowash/>

Wisconsin Department of Natural Resources  
*Wisconsin Trail Etiquette & Safety Guidelines*  
<http://dnr.wi.gov/files/PDF/pubs/pr/PR0472.pdf>

Wisconsin Department of Transportation *Wisconsin Bicycle Facility Design Handbook*  
<http://www.dot.wisconsin.gov/projects/state/docs/bike-facility.pdf>

Wisconsin Department of Transportation *WIS 15 Improvement Project* website  
<http://www.dot.wisconsin.gov/projects/neregion/15/>







## **APPENDIX D**

### **FUNDING RESOURCES**

## APPENDIX D: FUNDING RESOURCES

| <b>Program</b>   | <b>Purpose</b>   | <b>Details</b>                                    | <b>Deadlines</b> | <b>Notes</b>  | <b>Agency</b> | <b>Contact</b>                                   |
|--|--|---|------------------|---|---------------|--|
| <b>Wisconsin Stewardship Programs</b>                          |  |   |                  |   |               |  |
| Acquisition of Development Rights                              | Acquire development rights for nature-based outdoor recreation areas and activities  | \$1.6 M available per year<br><br>50% local match | May 1            | Funds available for areas where restrictions on residential, commercial & industrial development<br><br>May include enhancements of outdoor recreation                      | WDNR          | Chris Halbur, Northeast District<br>920-662-5121 |
| Aids for the Acquisition and Development of Local Parks (ADLP) | Acquire or develop public, nature-based outdoor recreation areas and facilities  | \$4 M available per year<br>50% local match       | May 1            | A comprehensive outdoor recreation plan is required<br><br>Priority for land acquisition Projects must comply with ADA  | WDNR          | Chris Halbur, Northeast District<br>920-662-5121 |
| Urban Greenspace Program (UGP)                                 | Acquire land to provide natural space within or near urban areas, or to protect scenic or ecological features  | \$1.6 M available per year<br>50% local match     | May 1            | A comprehensive outdoor recreation plan is required.<br><br>Projects must comply with ADA   | WDNR          | Chris Halbur, Northeast District<br>920-662-5121 |
| Urban Rivers Grant Program (URGP)                              | Acquire land, or rights in lands, adjacent to urban rivers for the purpose of preserving or restoring them for economic revitalization or nature-based outdoor recreation activities | \$800,000 available per year<br>50% local match   | May 1            | A comprehensive outdoor recreation plan is required.<br><br>Projects must comply with ADA   | WDNR          | Chris Halbur, Northeast District<br>920-662-5121 |
| Land & Water Conservation Fund (LAWCON)                        | Acquire or develop public outdoor recreation areas and facilities  | 50% local match per project                       | May 1            | A comprehensive outdoor recreation plan is required   | WDNR          | Chris Halbur, Northeast District<br>920-662-5121 |
| Recreational Trails Act (RTA)                                  | Provide funds for maintenance, development, rehabilitation, and acquisition of non-motorized multi-trails  | 50% local match per project                       | May 1            | Funds may only be used on trails identified in or that further a goal of a local, county or state plan<br>Funds may be used on trails that are in SCORP a state park plan - | WDNR          | Chris Halbur, Northeast District<br>920-662-5121 |

| <b>Program</b>  | <b>Purpose</b>  | <b>Details</b>  | <b>Deadlines</b> | <b>Notes</b>   | <b>Agency</b>               | <b>Contact</b>   |
|---|---|---|------------------|--|-----------------------------|--|
| Wisconsin Main Street Community Program                         | Comprehensive downtown revitalization program, which includes Streetscape improvements  | Wisc. Dept. of Commerce                                     | No Date          | General downtown Program.<br><br>May benefit trail enhancements through streetscaping  | National Main Street Center | Bureau of Downtown Development<br>608-266-7531   |
| Surface Transportation Environment Cooperative Research Program | Evaluate transportation control measures. Improve understanding of transportation demand factors. Develop performance indicators that will facilitate the analysis of transportation alternatives | 20% local match per project                                 | No Date          | Money available for: development of national bicycle safety education curriculum grants to a national not-for-profit organization engages in promoting bicycle and pedestrian safety study of the safety issues attendant to the transportation of children to and from school by various transportation | FHWA                        | US Dept. of Transportation<br>202-366-4000   |
| Urban Forestry Grants   | Assistance for tree maintenance, planting, and public awareness   | \$1,000 to \$25,00 of grants awarded with a 50% local match | October 1        | Funding is prioritized for communities needing to develop an urban forestry plan, needing worker training, and needing to conduct a street tree inventory  | WDNR Urban Forestry         | <b>Tracy Salisbury</b><br>2984 Shawano Ave<br>Green Bay, WI 54313-6727<br><b>Phone:</b> 920-662-5450<br><b>Fax:</b> 920-662-5159<br><b>Email:</b> <a href="mailto:Tracy.Salisbury@Wisconsin.gov">Tracy.Salisbury@Wisconsin.gov</a> |
|   |   |   |                  |  |                             |  |

| Program  | Purpose   | Details   | Deadlines    | Notes   | Agency                   | Contact   |
|--|---|---|--------------|---|--------------------------|---|
| <b>Transportation Grants</b>                     |   |   |              |   |                          |   |
| <b>Transportation Alternatives Program (TAP)</b> | Provide facilities for pedestrians and bicyclists. Provides funding for rehabilitating and operating historic transportation buildings and structures as well as "Main Streets" | Funded with TE-Map 21<br><br>20% required match | February     | Not a grant program. 80% of funds are reimbursed if all federal guidelines are met. Project must relate to surface transportation.<br><br>Construction projects must be over \$100,000<br><br>Non-construction projects must be over \$25,000                           | WisDOT                   | Tressie Kamp<br><br>608-266-3973<br><br><a href="http://www.fhwa.dot.gov/map21">http://www.fhwa.dot.gov/map21</a> |
| Surface Discretionary Grant Program (STP-D)      | Provides flexible funds that can be spent on a wide variety of projects, including roadway, bridges, transit facilities, and bike and pedestrian facilities                     | Funded with TE-Map 21<br><br>20% match required | February     | Any project that fosters alternatives to single-occupancy vehicle trips<br><br>Facilities for pedestrians and bicyclists<br><br>System-wide bicycle planning<br><br>Construction projects must be over \$100,000<br><br>Non-construction projects must be over \$25,000 | WisDOT                   | Tressie Kamp<br><br>608-266-3973  |
| Safe Routes to School Grant Program (SRTS)       | Intended to combat childhood obesity and reestablish walking and biking to school   | Funded with TE-Map 21                           | December     | Funding for bicycle and pedestrian facilities<br><br>Funding for pedestrian and bicycle safety and programs   | WisDOT                   | Renee Callaway<br><br>608-266-3973  |
| Section 5309 (old Section 3 discretionary funds) | Transit capital projects; included intermodal facilities such as bicycle racks on buses and bicycle parking at transit stations;  | 20% local match per project                     | Early Spring | Funding for this program is allocated on a discretionary basis<br><br>Congress reserves money each year<br><br>Administration can pick the projects   | WisDOT Bureau of Transit | Ron Morse<br><br>608-266-1650   |

**Community Grants:**

Wisconsin Community Fund: These are for non-profits groups.

<http://www.forwardci.org/wisconsin-community-fund>

<http://www.cffoxvalley.org/page.aspx?pid=873>

**Sports Field Grants:**

<https://www.responsiblesports.com/programs/grants/rules>

<http://www.hksportsfields.com/sports-complex-financing/>

**Playground Grants:**

<http://www.bciburke.com/grants.html>

<http://www.miracle-recreation.com/purchase-options/playground-grants.html>

<http://www.playlsi.com/Research-Plan/Playground-Planning/Financing/Playground-Grants/Pages/Playground-Grants.aspx>

<http://korkatblog.com/playground-grants/>

<http://www.letsplay.com/get-involved/about-playground-grants?gclid=CJzZ2N792bgCFZA-Mgod6AsA6Q>

**Educational:** (Could use for landscaping also)

<http://eeinwisconsin.org/core/item/topic.aspx?s=0.0.0.2209&tid=85010>

**National:** The National Parks and Recreation Association has fundraising ideas and resources along with grants for parks.

<http://www.nrpa.org/fundraising-resources/>



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